



# Public Document Pack

## Cambridge City Council

### DEVELOPMENT PLAN SCRUTINY SUB-COMMITTEE

**To:** **Scrutiny Sub Committee Members:** Councillors Sarris (Chair), Gawthrope (Vice-Chair), Ashton, Baigent, C. Smart and Tunnacliffe

**Alternates :** Councillors Reid and M. Smart

**Executive Councillor for Planning Policy and Transport:** Councillor Blencowe

*Despatched: Friday, 4 March 2016*

**Date:** Monday, 14 March 2016

**Time:** 5.30 pm

**Venue:** Committee Room 1 & 2 - Guildhall

**Contact:** Claire Tunnicliffe

**Direct Dial:** 01223 457013

### AGENDA

#### 1 Apologies

To receive any apologies for absence.

#### 2 Declarations of Interest

Members are asked to declare at this stage any interests, which they may have in any of the following items on the agenda. If any member is unsure whether or not they should declare an interest on a particular matter, they are requested to seek advice from the Head of Legal Services **before** the meeting.

#### 3 Minutes

*(Pages 5 - 10)*

To approve the minutes of the meeting on 8 December 2015.

- 4      Public Questions**
- 5      Cambridge Local Plan - Proposed Modifications - Report on Consultation March 2016**  
*Documents attached separately*
- 6      Mill Road Depot Planning and Development Brief Supplementary Planning Document**  
*(Pages 11 - 102)*
- 7      Planning Guidance Note for Park Street Car Park**  
*(Pages 103 - 130)*
- 8      New Museums Site Development Framework SPD**  
*(Pages 131 - 242)*



# Information for the Public

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The meeting is in the Guildhall on the Market Square (CB2 3QJ).

Between 9 a.m. and 5 p.m. the building is accessible via Peas Hill, Guildhall Street and the Market Square entrances.

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**DEVELOPMENT PLAN SCRUTINY SUB-COMMITTEE**

8 December 2015

4.30 - 5.00 pm

**Present:** Councillors Sarris (Chair), Blencowe, Baigent and C. Smart and M Smart

Executive Councillor for Planning Policy and Transport: Councillor Blencowe

**Officers:**

Urban Design and Conservation Manager: Glen Richards

Planning Policy Manager, Environment: Sara Saunders

Principal Planning Policy Officer: Joanna Gilbert-Wooldridge

Planning Policy Officer: Frances Schulz

Committee Manager: Claire Tunnicliffe

**FOR THE INFORMATION OF THE COUNCIL****15/39/DPSSC Apologies**

Apologies were received from Councillors Ashton, Gawthroe and Tunnacliffe.

Councillor M Smart attended as an alternate for Councillor Gawthroe.

**15/40/DPSSC Declarations of Interest**

Name	Item	Reason
Councillor Baigent	15/44/DPSSC	Personal: Lives opposite the proposed development. Withdrew from discussion and room, and did not vote.

**15/41/DPSSC Minutes**

Councillor C Smart requested that under 15/37/DPSSC: (*Cambridge Northern Fringe East Area Action Plan, Issues and Options Consultation Feedback*), to delete the following text and replace with the following (deleted text ~~struck through~~ additional text underlined).

**Any Alternative Options Considered and Rejected**

~~Not applicable~~ Alternative options considered as outlined in Appendix A of the Officer's report.

Following these amendments the minutes of 17 November 2015 were then approved and signed by the Chair as a correct record.

**15/42/DPSSC Re-ordering of the Agenda**

Under paragraph 4.2.1 of the Council Procedure Rules, the Chair used his discretion to alter the order of the agenda items. However, for ease of the reader, these minutes will follow the order of the agenda.

**15/43/DPSSC Public Questions**

There were no public questions.

**15/44/DPSSC Ridgeons Site, Cromwell Road****Matter for Decision**

To consider the draft Ridgeons Cromwell Road Planning and Development Brief Supplementary Planning Document (SPD) (Appendix A) produced for public consultation. The document outlined the aspirations of the site, key issues, constraints and opportunities that would influence how future development on the site would take place. Detailed local and stakeholder consultation had taken place throughout 2015 and assisted with drafting of the brief.

**Decision of Executive Councillor for Planning Policy and Transport**

- i. Agreed the content of the draft Ridgeons Cromwell Road Draft Planning and Development Brief SPD (Appendix A of the Officer's report)
- ii. Agreed that if any amendments were necessary, these should be agreed by the Executive Councillor in consultation with Chair and Spokes of Development Plan Scrutiny Sub Committee
- iii. Approved the draft SPD for public consultation from 18 January to 29 February 2016
- iv. Approved the consultation arrangements as set out in paragraphs 3.10 to 3.12 and the proposed schedule of consultees in Appendix B of the Officer's report.

**Reason for the Decision**

As set out in the Officer's report.

**Any Alternative Options Considered and Rejected**

Not applicable.

**Scrutiny Considerations**

The Committee received a report from the Urban Design and Conservation Manager.

The report referred to the adopted Cambridge Local Plan 2006 which had allocated the southern part of the Ridgeons site for housing (site allocation 5.14) and the emerging Local Plan which allocated the entire site for 245 homes (R12). The proposals scheduled in the emerging Local Plan states that 'the site promoters would be expected to prepare a Planning and Development Brief for the site demonstrating how development would successfully integrate with the existing residential area as well as addressing the constraints and opportunities of the site. Scale, massing and density considerations in the design and disposition of new and existing housing would be expected to have regard to the character of the existing area'.

**Comments from the Sub Committee**

- i. Stated that the Chisholm Trail consultation had included Cromwell Road and Brampton Road as alternative routes but there seemed to be no reference to the Chisholm Trail and Cromwell Road in the Officer's report.
- ii. Recommended that references to Veritas Court in the consultation document should be changed Pym Court.
- iii. The map on page 63 of the agenda pack showed Thoday Street in Petersfield which was incorrect.
- iv. Queried what the area of development would be called and should be in keeping with the area.
- v. Suggested the development could be named after Katherine Chidley, a female leader of the Levellers.
- vi. Sought reassurance that the relevant stakeholders concerning the Chisholm Trail had been contacted as part of the process when compiling the consultation document for this development.
- vii. Sought clarity about the approach to car parking standards.

Officers stated the following:

- i. Highlighted that page 55 of Appendix A referenced the Chisholm Trail with the proposed route to the North of the development.
- ii. The County Council's preferred route was access to the East which connected to Brampton Street but it was also intended to have a side spur to run through the site to link to Crowell Road.
- iii. Agreed to check the rewording, and potentially amend, from Veritas Court to Pym Court.
- iv. Names of the development had not yet been discussed.
- v. Confirmed that the map would be updated with the correct names as required.
- vi. Agents on behalf of Ridgeons and City and County Officers had worked with Chisholm Trail representatives and would continue to do so. It was important to the developers that cyclists would be able to use and have access through the site.
- vii. Confirmed that the approach to car parking standards would be in accordance with the Local Plan and a section relating to car parking standards is set out in the draft SPD.

The Committee **resolved** unanimously to endorse the recommendations.

The Executive Councillor approved the recommendations.

**Conflicts of Interest Declared by the Executive Councillor (and any Dispensations Granted).**

No conflicts of Interest were declared by the Executive Councillor.

**15/45/DPSSC    Annual Monitoring Report 2015**

**Matter for Decision**

To consider the Annual Monitoring Report.

This is an important part of the planning process, providing feedback on the performance of development plan policies in terms of their use and implementation. The Council is required to produce an Annual Monitoring Report (AMR) at least on a yearly basis.

## **Decision of Executive Councillor for Planning Policy and Transport**

- i. Agreed the content of the AMR (Appendix A)
- ii. Agreed that if any amendments are necessary, these should be agreed by the Executive Councillor in consultation with Chair and Spokes of Development Plan Scrutiny Sub Committee.

### **Reason for the Decision**

As set out in the Officer's report.

### **Any Alternative Options Considered and Rejected**

Not applicable.

### **Scrutiny Considerations**

The Committee received a report from the Planning Policy Officer.

The report referred to how comprehensive monitoring was essential in order to establish whether the Council was succeeding in promoting and managing the future development of Cambridge. The Localism Act 2011 and Section 34 of the Town and Country Planning Act (Local Planning) (England) Regulations 2012 had established the statutory need for monitoring reports.

The Planning Policy Officer highlighted the tabled amendments to paragraphs 8.26 and 8.27 of the report due to the amended data that had been received from Cambridgeshire County Council.

### Comments from the Sub Committee

- i. Requested confirmation on how many Public Houses were on unprotected sites in the City.
- ii. Enquired how the figure of 23.8% (key services) under the heading of the Accessibility of Services referenced in 8.27 of the amendment sheet had been calculated.
- iii. Asked if the £78 million spend in the local area per annum generated by Language schools referenced in 8.3 of the Officer's report related to student spending.

### Officers stated the following:

- i. The number of safeguarded public houses currently stood at a total of 102. The Council has produced Interim Planning Policy Guidance

(IPPG) on the Protection of Public Houses, and this document is used to deal with planning applications affecting public houses. The IPPG confirmed that safeguarded public houses are those which were operating as public houses on 21 July 2006 (when the Cambridge Local Plan 2006 was adopted). As sites came forward for Public House use, the IPPG would continue to be used, until replaced by policy in the emerging Local Plan, and the Annual Monitoring Report would be updated as any new public houses come forward or any public houses close.

- ii. The figure of 23.8% signified the percentage of new housing developments which were within 15 minutes public transport or walking distance of six key services. The six key services included retail centre, area of employment, primary school, secondary school, GP surgery and hospital with outpatients. Information had been taken from new developments completed within the monitoring year.
- iii. No further data could be obtained from the County Council regarding the population in Cambridge who were within fifteen minutes of the six key services when using public transport.
- iv. The £78 million related to the fees and income generated from the Language Schools and spend in the local area generated from the presence of language schools.

The Committee **resolved** unanimously to endorse the recommendations.

The Executive Councillor approved the recommendations.

**Conflicts of Interest Declared by the Executive Councillor (and any Dispensations Granted).**

No conflicts of Interest were declared by the Executive Councillor.

The meeting ended at 5.00 pm

**CHAIR**





To: Executive Councillor for Planning Policy and Transport  
Report by: Director of Environment  
Relevant scrutiny committee: Development Plan Scrutiny Sub Committee 14/3/2016  
Wards affected: Petersfield Ward

### **MILL ROAD DEPOT DRAFT PLANNING AND DEVELOPMENT BRIEF SUPPLEMENTARY PLANNING DOCUMENT**

Not a Key Decision

#### **1. Executive summary**

- 1.1 The submitted Cambridge Local Plan (2014) allocates the Mill Road Depot and adjoining properties for approximately 167 homes (R10). The allocation is located within the proposed policy area for Policy 23: Mill Road Opportunity Area. The City Council as the Local Planning Authority has been working in partnership with Allies and Morrison<sup>1</sup> to prepare a planning and development brief. The planning and development brief will help guide the redevelopment of the site for housing and will provide greater certainty and detail to support delivery of the allocation.
- 1.2 In order to ensure sufficient weight can be given to such a brief, the document is being prepared in the form of a Supplementary Planning Document (SPD).
- 1.3 The draft Mill Road Depot Planning and Development Brief SPD (Appendix A) has been produced for public consultation. The document outlines the aspirations for the site, as well as the key issues, constraints and opportunities that will influence how future development on the site will take place. Detailed local and stakeholder consultation has taken place on two occasions, which has helped inform the drafting of the brief.
- 1.4 A six week public consultation is proposed to take place after the local elections in May 2016.

#### **2. Recommendations**

- 2.1 This report is being submitted to the Development Plan Scrutiny Sub-Committee for prior consideration and comment before decision by the Executive Councillor for Planning Policy and Transport.
- 2.2 The Executive Councillor is recommended:

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<sup>1</sup> Allies and Morrison have been appointed on behalf of the City Council as a land owner.  
Report Page No: 1

- a) To agree the content of the draft Mill Road Depot Draft Planning and Development Brief SPD (Appendix A);
- b) To agree that if any amendments are necessary, these should be agreed by the Executive Councillor in consultation with Chair and Spokes of Development Plan Scrutiny Sub Committee;
- c) To approve the draft SPD for public consultation to commence in May 2016;
- d) To approve the consultation arrangements as set out in paragraphs 3.10 to 3.11 and the proposed schedule of consultees in Appendix B.

### 3. Background

- 3.1 Mill Road Depot is the main site for many of Cambridge City Council's services, including waste disposal, maintenance, storage, and a garage servicing centre for its vehicles. It also incorporates council office space, commercial lettings, two community facilities and leased garages. The council's decision in October 2014 to relocate the Depot to Waterbeach, following the creation of a single shared waste facility with South Cambridgeshire District Council (SCDC), has freed up the site for redevelopment. The council has already reduced the activities carried out on site and plan to vacate the site and buildings in the near future. The site presents a good opportunity to provide more homes to meet the growing demand for housing in Cambridge.
- 3.2 The site is 2.7 hectares in area and has the potential to deliver 167 dwellings, as identified in the Strategic Housing Land Availability Assessment (SHLAA) and Site Allocation R10 in the submission Cambridge Local Plan (2014). The allocation is located within the proposed policy area for Policy 23: Mill Road Opportunity Area. More specifically to the site's implementation, the proposals schedule in the emerging Local Plan states the following: *"The site promoters will be expected to prepare a planning and development brief for the site demonstrating how development will successfully integrate with the existing residential area as well as addressing the constraints and opportunities of the site. Scale, massing and density considerations in the design and disposition of new and existing housing will be expected to have regard to the character of the existing area"*.
- 3.3 The City Council as the Local Planning Authority has been working in partnership with Allies and Morrison<sup>2</sup> to prepare a planning and development brief for the allocation. As part of this process, there have been two stages of informal local consultation held as well as input from key local stakeholders. In November 2015, the council hosted a workshop in the local area of the site which focused on issues and priorities for the future development of the site. This event provided an opportunity for local residents and stakeholders to make clear their aspirations (and concerns) for any redevelopment of the site and helped inform subsequent work by consultants and council officers.
- 3.4 The second stage of consultation took place in January 2016 and involved consultation on draft planning and design principles for the site. Again a workshop format was used and a significant amount of feedback was collected once more, this time on more detailed ideas for various "themes" to guide development proposals (landscape and open space, movement, built form, etc.). All City and County Council members of Romsey and Petersfield wards were invited to both consultation events.

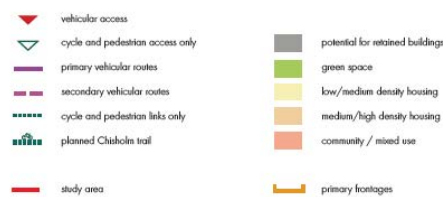
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<sup>2</sup> Allies and Morrison have been appointed on behalf of the City Council as a land owner.

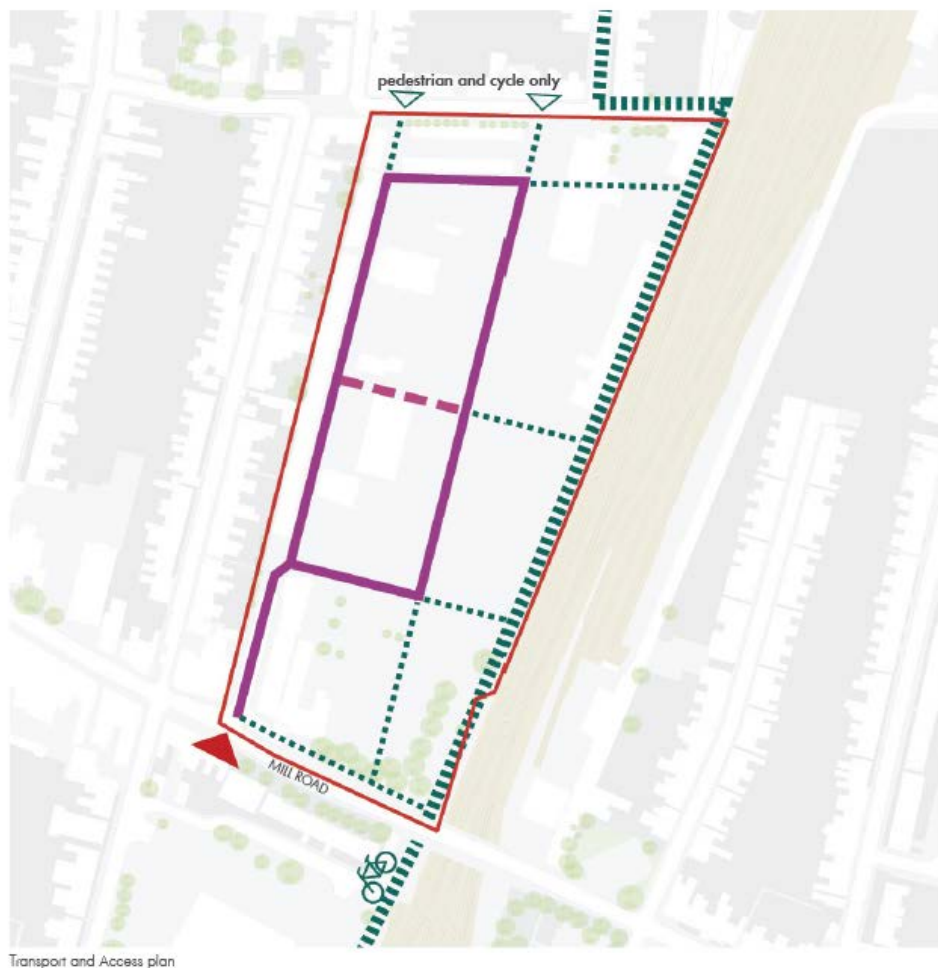
3.5 In addition a number of stakeholder groups were invited such as the Mill Road Society, Petersfield Area Community Trust (PACT), Mill Road Co-ordinator, Romsey Garden Club, Romsey Garden Project, Cam Cycle, Cambridge Past, Present and Future, Glisson Road and Tenison Road Area Residents Association (GTARA), South Petersfield Residents Association (SoPRA), Cambridge Association of Architects, Cambridge Ethnic Community Forum (CECF) and the Argyle Street Housing Co-operative (ASH co-op).

3.3 The emerging Mill Road Depot SPD has sought to address comments received to date constructively and creatively and balance the practical need of site delivery with the context of the local housing market and the council's own objectives. The emerging Mill Road Depot SPD includes the following key parts:

- a) A series of "development principles" which will guide future development of the site. These development principles set out "high level" requirements to which development should aspire and against which planning applications can be assessed. The principles are not generic but instead are tailored to ensure development responds positively to the existing constraints and opportunities of the site and surrounding location. The development principles relate to the following:
  - a. Access and permeability
  - b. Housing-led mix of uses
  - c. Design
  - d. Open spaces/environment
- b) An overall draft "framework plan" as noted below (in effect a composite of the various plans which follow):



- c) A transport and access plan which sets out key routes for all modes, as shown below. Note that the development is expected to include part of the future Chisholm Trail within its easterly edge. The main features of the access and movement diagram are a single, all-vehicular access taken from Mill Road and a separate/secondary access for emergency vehicles, cycles and pedestrians only from Hooper Street, in addition to a north-south oriented circular street through the site with secondary routes which connect between and off this route:

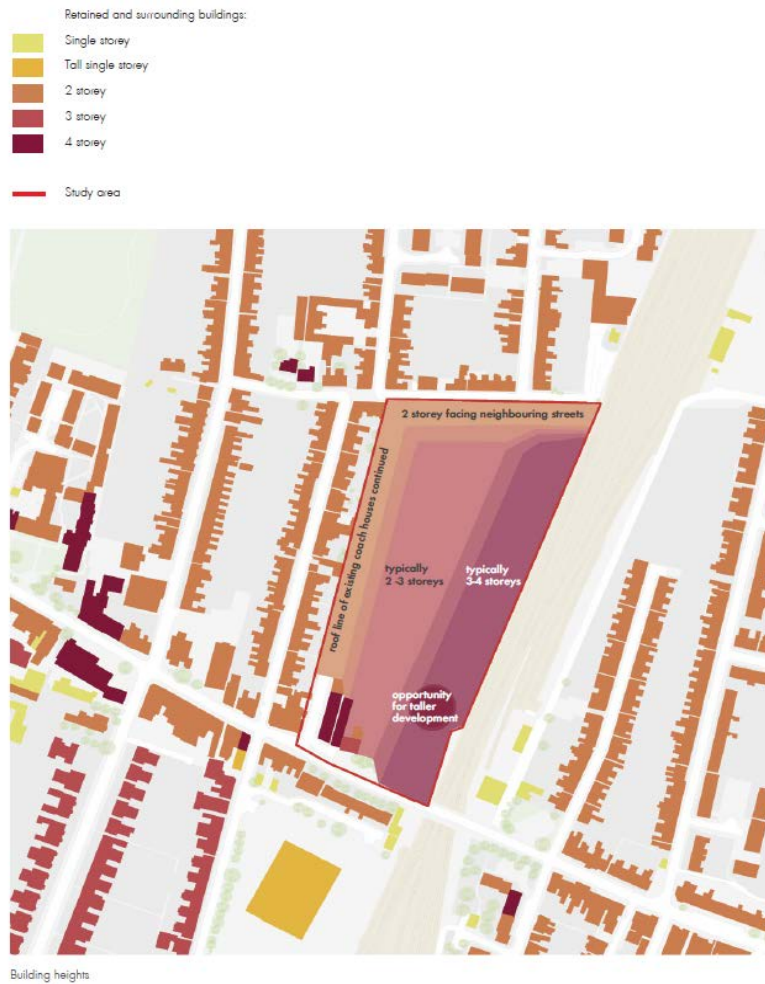


- d) An open space plan which sets out different types and sizes of green space in various locations across the site. The main spaces would comprise two larger areas of open space, one in the south-east corner and one towards the north end of the site; two linear green spaces, one within the site and one twinned with the future Chisholm Trail on the eastern edge of the site; and one small space to the rear of the existing Gatehouse.



- e) The final key part of the framework is a building heights plan. Building heights are a sensitive subject in existing communities in Cambridge. The emerging Mill Road Depot SPD seeks to ensure that where new development abuts existing development e.g. near Kingston Street and Hooper Street, building heights should be typically kept to 2 storeys maximum. Along the rail line building height may be able to rise to 3-4 storeys to provide a strong edge to the rail line but also to provide and appropriate location flats so that a variety of accommodation can be delivered overall across the site. Taller building forms along the rail line also provide a degree of visual and noise attenuation to rail traffic. There is reference to an “opportunity for taller development” in one location only towards the south-eastern end of the site, which is meant to signify that a building could be located here at 5-storeys in height, however this would be subject to clear justification and excellent design.

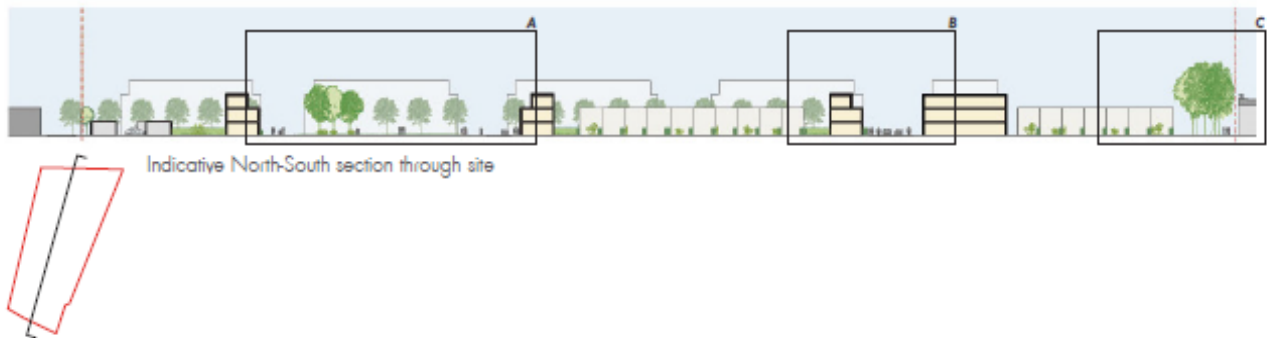




- f) An illustrative masterplan is included in the draft Mill Road Depot SPD. This plan must be read only for what it is, an illustration. It shows one way in which the framework plan and higher level “parameters” of movement, open space, etc. could be delivered on site:



Finally, the following indicative site section shows how the proposed scale and mass the site could appear when viewed from the west e.g. looking in an easterly direction when positioned in the middle of the site:



3.6 Residents and stakeholders raised several issues during the consultation events which the consultants and council officers have considered further and sought to address in the draft Mill Road Depot SPD. While not an exclusive list of matters raised at workshops, these issues do represent the main “themes” which came up in most in discussions of working groups at the events. The issues include:

- a) integration of community facilities
- b) open space provision
- c) building heights
- d) parking
- e) site access
- f) level of affordable housing
- g) future of existing leased garages

3.7 Taking each of the above issues in turn:

- a) There appeared to be a general consensus at the workshops about the need to maintain some form of community use on the site. There was also recognition that the Women's Resource Centre, currently housed in a building on the north-east corner of the site, would require future re-location (whether off or on-site would need to be determined). Many felt that community uses, such as the Bharat Bhavan use of the old library, are a good use of the more heritage characterful buildings at the southern end of the site (the Gatehouse and former library, for example). It is clear however that the SPD cannot be overly specific at this stage about what community uses may or may not be appropriate on site. Further work will be required in the form of a brief to help inform any community uses planned alongside the delivery of housing.
- b) Open space was a subject of considerable interest at both consultation events. A dispersion of good quality, accessible open space within the site was generally supported, though concern was expressed about the potential for anti-social activity occurring in some of these areas. To avoid this occurring, such spaces will need to be both inviting to use, well lit and well overlooked by future development. The protection of existing trees and room for future trees was seen as important. Use of green spaces for food growing and communal gardens was also encouraged.
- c) Building heights were discussed at each event. An approximate building height range of between 2-4 residential storeys on the site was considered acceptable, with a caveat that lower building heights should be required adjacent existing residential boundaries. The brief includes this height range,

with potential for taller (4 storey) flats on the east side of the site along the rail line and one localised opportunity for a taller building (5-storeys) in the south-eastern corner of the site, subject to design.

- d) Parking is a topic that always raises public interest and the two workshops on the Mill Road Depot were no exception. Many workshop attendees were strongly encouraging a car-free development on the site, while others were encouraging low car parking ratios e.g. less than 0.6 spaces per unit. Other comments raised the notion of parking underground. In all cases, the management of car parking was seen as key to any successful scheme and to avoid conflict with surrounding areas. Rather than set out a specific parking ratio in the SPD, the text used in the draft SPD states that new development should promote lower car parking ratios overall.
- e) Site access is a challenge for any new development that fronts Mill Road, and it is particularly challenging in a location next to a bridge over a rail line. The existing visual splay for drivers exiting Mill Road Depot is particularly poor. The County Council as highway authority do not consider the existing access to be suitable for residential use, hence a new solution to cater for residential development will be necessary. The City Council appointed external consultants to review options for improving this junction and it has been recommended that changes to the junction as well as improvements to create better sight lines looking east will be necessary. Further detailed work will be necessary to refine the design and cost for any such junction.
- f) The level of affordable housing was discussed at both workshops and there was a good level of support for a balance of house types and tenures, including for the development of affordable housing. A preference appeared to emerge that the council should retain freehold ownership of the site and pursue a range of affordable house types on site.
- g) The existing privately leased garages at the northern end of the site were the subject of interest at consultation events. Given the relatively long lease that remains on some of these garages (understood to be up to 60 years in some cases), it is clear that there is concern over the potential loss of the use of the garages for some people. The framework plan and related plans in the SPD are therefore tailored to enable the garages to remain at least in the short/medium term and, in the event of the termination of garage leases and removal of the garage structures, the land upon which they are located could then be re-developed for housing fronting Hooper Street as part of the wider redevelopment.

- 3.8 Planning obligations will be necessary as part of the re-development of the Mill Road Depot. The wording as proposed in the draft SPD is more general in this regard, but ultimately any future developer will have to agree the details of planning obligations at planning application stages.

### **Consultation Arrangements**

- 3.9 It is proposed that a public consultation takes place on the draft SPD after the local elections in May. This will be for six weeks, running from mid-May to late June 2016, with precise dates to be confirmed in due course.

- 3.10 In line with the council's adopted Statement of Community Involvement, the proposed consultation arrangements will be as follows:

- Letters / e-mails including consultation details to be sent to statutory and general consultees.



- The draft SPD to be made available to view at the following locations:
  - The council's website – [www.cambridge.gov.uk](http://www.cambridge.gov.uk)
  - Cambridge City Council's Customer Service Centre, Mandela House, 4 Regent Street, Cambridge, CB1 0JH.
  - Cambridge Central Library.
- An on-line consultation system will be available on the council's websites in order for people to respond directly via the internet. Hard copies of the response form will be made available at the council's Customer Service Centre for those who do not have access to the internet.
- An exhibition will be held in the local area during the course of the consultation, the date and location of which has yet to be finalised. However, once arranged the event will be advertised in the local area.

3.11 A Sustainability Assessment and Habitats Regulations Assessment Screening Report have been carried out and consulted upon for the emerging Cambridge Local Plan 2014. This consultation took place between 19 July and 30 September 2013. These documents, along with other supporting documents will also be made available to view during this consultation. As the draft SPD supports the Cambridge Local Plan, there is no further need to undertake a separate Sustainability Appraisal or Habitats Regulations Assessment for this document, although screening reports have been completed and will be made available during the consultation.

## **Next Steps**

- 3.12 The representations received will then be used to help guide the development of the draft SPD and will be reported along with the final version of the SPD.
- 3.13 The SPD will be adopted at the same time as, or shortly after, the Local Plan is adopted. It cannot be adopted before the Local Plan is adopted as it is the Local Plan that provides the policy basis for this site allocation.

## **4. Implications**

### **Financial Implications**

- 4.1 There are no significant financial issues arising from the preparation of this SPD. Project and staffing resources are already committed through the budget and service plan process. Funding for consultation is allowed for in existing consultation budgets.

### **Staffing Implications**

- 4.2 There are no direct staffing implications arising from this report. The development of the SPD is already included in existing work plans.

### **Equality and Poverty Implications**

- 4.3 The SPD, once adopted, will have a positive impact by ensuring the site is integrated into the wider area. New development within the site should provide access for people with limited mobility.

## **Environmental Implications**

- 4.4 The redevelopment of the Mill Road Depot should provide for the development of energy efficient buildings as well as the inclusion of renewable and low carbon energy generation.

## **Consultation**

- 4.5 Consultation arrangements are set out in other parts of this report and are consistent with the council's Code of best practice on consultation and community engagement and Statement of Community Involvement 2013.

## **Community Safety**

- 4.6 There are no direct community safety implications arising from this report.

## **5. Background papers**

- 5.1 These background papers were used in the preparation of this report:
- Cambridge Local Plan 2014: Proposed Submission (as amended)  
<https://www.cambridge.gov.uk/local-plan-review-proposed-submission-consultation>
  - Event Record prepared by Allies and Morrison Architects (February 2016)
  - [Mill Road Depot Surface Water Drainage](#) (Cambridge City Council, 2016)
  - [Surface water storage requirements for sites](#) (HR Wallingford, 2016)
  - [Indicative Risk of Contamination](#) (Cambridge City Council, 2016)

## **6. Appendices**

- Appendix A: Mill Road Depot Draft Planning and Development Brief Supplementary Planning Document
- Appendix B: Proposed Statement of Consultation

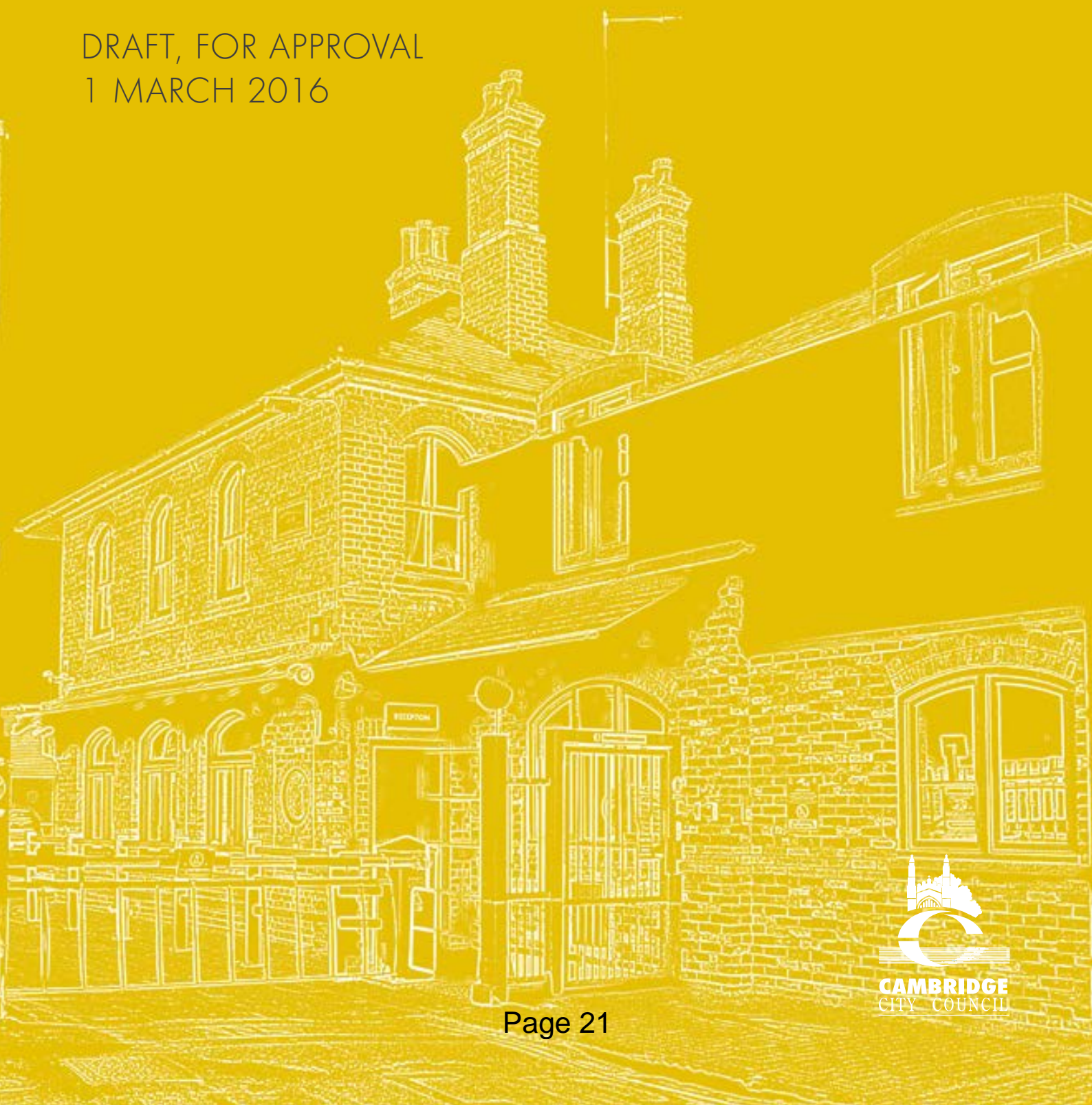
## **7. Inspection of papers**

To inspect the background papers or if you have a query on the report please contact:

Author's Name: Glen Richardson  
Author's Phone Number: 01223 457374  
Author's Email: [glen.richardson@cambridge.gov.uk](mailto:glen.richardson@cambridge.gov.uk)

# MILL ROAD DEPOT SUPPLEMENTARY PLANNING DOCUMENT

DRAFT, FOR APPROVAL  
1 MARCH 2016





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**Fig 1** Aerial photograph with Mill Road Depot site highlighted for context



# 1 INTRODUCTION

## 1.1 BACKGROUND

- 1.1.1 Mill Road Depot (see figure 1) is the main site for many of Cambridge City Council's services, including waste disposal, maintenance, storage, and a garage servicing centre for its vehicles. It also incorporates Council office space, commercial lettings, two community facilities and leased garages. The Council's decision in October 2014 to relocate the Depot to Waterbeach, following the creation of a single shared waste facility with South Cambridgeshire District Council (SCDC), has freed up the site for redevelopment. The Council has already reduced the activities carried out on site and plan to vacate the site and buildings in the near future.
- 1.1.2 The site is 2.7 hectares in area and has the potential to deliver 167 dwellings, as identified in Site Allocation R10 (see figure 2) in the Proposed Submission Cambridge Local Plan (2014). The site presents a good opportunity to provide more homes to meet the growing demand for housing in Cambridge.
- 1.1.3 Mill Road Depot is situated centrally within the Mill Road Opportunity Area, identified in Policy 23 of the emerging Cambridge Local Plan. Any potential re-development of the depot site should support and strengthen the character and distinctive local community of the Mill Road area.

### Process of preparation

- 1.1.4 Cambridge City Council appointed Allies and Morrison Architects to prepare a draft planning and development brief to the editorial control of the council as "client". As part of this process, there have been two stages of informal local consultation held as well as input from key local stakeholders."
- 1.1.5 In November, 2015, a workshop in the local area of the site was organised to focus on the issues and priorities for the future development of the site. This event provided an opportunity for local residents and stakeholders to make clear their aspirations (and concerns) for any redevelopment of the site and helped inform subsequent work by consultants and council officers."
- 1.1.6 The second stage of consultation involved consultation on draft planning and design principles for the site. Again a workshop format was used and a significant amount of feedback was collected once more, this time on more detailed ideas for various "themes" to guide development proposals."
- 1.1.7 The events were captured in a consultation statement which is provided on the council's website. The draft SPD has sought to address these comments constructively and creatively and at the same time balance the practical need of site delivery in the context of the local housing market and the council's own objectives.



**Fig2** Ordnance survey of site (extract from Local Plan, 2014)



## 1.2 PURPOSE AND SCOPE

- 1.2.1 This Supplementary Planning Document (SPD) is a planning guidance document which will support policy in the emerging Cambridge Local Plan. SPDs are in one of two categories: the first is guidance supporting a city-wide objective, such as the Affordable Housing SPD; the second is guidance for a specific site or area. This SPD falls into the second category.
- 1.2.2 The SPD has been prepared in the form of a Planning and Development Brief to provide planning and design guidance to developers, and to help guide the preparation and assessment of future planning applications on the Mill Road Depot site. As such, this document will form a material consideration, which will be taken into account by Cambridge City Council when determining any future planning application for the site. In addition, all proposals for the Mill Road Depot site will have to comply with the policies in the adopted Cambridge Local Plan 2014, once adopted.
- 1.2.3 This SPD has been prepared in line with the requirements of the Town and Country Planning (Local Planning) (England) Regulations 2012.

## 1.3 STRUCTURE

- 1.3.1 The development brief is structured as follows:
- Chapter 2: provides background context to the site and summarises the key constraints and opportunities.
  - Chapter 3: presents the vision statement and outlines the strategic design principles. This has been informed by the site analysis and consultation with the local community.
  - Chapter 4: sets out the parameters for future development of the Mill Road Depot site and gives guidance as to how these should be implemented.



**Fig 3** Cambridge City Council site allocation for Mill Road Depot site in the draft Cambridge Local Plan 2014

## 1.4 PLANNING POLICY CONTEXT

- 1.4.1 The emerging Cambridge Local Plan (2014) will guide future development in Cambridge to 2031. Mill Road Depot is identified as Site R10 in the Plan for housing as part of the Mill Road Opportunity Area and is shown in figure 3.
- 1.4.2 Following the Proposed Submission Consultation, the Schedule of Proposed Changes to the Cambridge Local Plan 2014: Draft Submission version (March 2014) sets out a description of the site and key requirements as follows:
- Abuts Mill Road Opportunity Area.
  - Contamination requires remediation.
  - Retain listed library building (in County Council ownership).
  - Open space requirements to reflect location in an area of open space deficiency.
  - Potential location for district energy centre.
  - Provide room for the Chisholm Trail.
  - Site will need careful review of highway access.
  - Vehicular access to be from Mill Road only, subject to detailed testing.
  - Range of housing typologies to be provided across the site.
  - The site promoters will be expected to prepare a planning and development brief for the site demonstrating how development will successfully integrate with the existing residential area as well as addressing the constraints and opportunities of the site. Scale, massing and density considerations in the design and disposition of new and existing housing will be expected to have regard to the character of the existing area.
- 1.4.3 The Mill Road Opportunity Area extends from Donkey Common in the west to Brookfields in the east. Development in opportunity areas should strengthen character and identity in key corridors of the city. Policy 23 of the Local Plan gives guidance to future development in the Mill Road Opportunity Area. It should:
- Contribute positively to the vitality and viability of the Mill Road district centres.
  - Enhance the area's unique historic character.
  - Implement a people focused placemaking approach.
- 1.4.4 Cambridge City Council submitted the Local Plan to the Secretary of State in March 2014 for independent examination and it is anticipated for adoption in 2017.

## LOCAL PLAN REVIEW

• Preparation and completion of evidence base	Spring 2011 to June 2012
• Issues and options consultation	June to July 2012
• Consultation on site options for development	January to February 2013
• Draft submission plan consultation	July to September 2013
• Submission	March 2014
• Examination	March 2014 onwards

Mill Road Depot site identified as draft site allocation R10 within Mill Road Opportunity Area. Commitment to preparation of planning and development brief was identified in the Schedule of Proposed Changes to the Cambridge Local Plan 2014: Draft Submission version (March 2014).

## PREPARATION OF MILL ROAD SPD

• Appointment of consultant team to prepare SPD	July 2015
• Analysis of site and background research	August to October 2015
• Consultation workshop 1: Themes & Factfinding	November 2015
• Development of guidance drawings	December to January 2016
• Consultation workshop 2: Test and review	January 2016
• Preparation of draft SPD	January to February 2016

### Future stages

• Approval to consult	March 2016
• Formal consultation stage - 6 weeks	Summer 2016
• Adoption of SPD	Concurrently following adoption of Cambridge Local Plan (estimated early 2017)

**Fig 4** Process of preparation - the current stage is shown in the box highlighted above (March 2016)

## 1.5 PROCESS OF PREPARATION

1.5.1 The Council consulted with local residents and a number of stakeholders concerning the depot site during the Proposed Submission consultation period for the Cambridge Local Plan in 2013. The following issues were raised:

- The need for open space and community facilities in the locality.
- Building heights and density to reflect the existing character of the area.
- Access to the site which should be from Mill Road only.
- Consideration of the wider impacts of new residential development on the surrounding area and communities, specifically social infrastructure and traffic flows.

1.5.2 The draft SPD has been informed by consultation with local stakeholders in the community. Two consultation workshops were held in November 2015 and January 2016 as an opportunity to provide feedback on the emerging proposals for the site. The consultation has shaped the key themes and design principles contained in this SPD.

1.5.3 In addition to this, a formal public consultation period of six weeks will be undertaken when stakeholders will have the opportunity to give feedback on the draft SPD. The consultation for this SPD is expected to commence in May 2016.

1.5.4 Comments received from the consultation period will inform the final version of this document, and amendments made to the draft SPD will also reflect:

- Any amendments to relevant policies in the adopted local plan; and
- Any government policy changes.

1.5.5 The overall process of preparing the SPD is summarised in the adjacent diagram, figure 4.

## 1.6 STATUS OF THIS DOCUMENT

1.6.1 This is the draft version of the Mill Road Depot SPD, and is anticipated to be agreed for consultation at Cambridge City Council Development Plan Scrutiny Sub-Committee by the Executive Councillor for Planning Policy and Transport on 14 March, 2016.

1.6.2 The SPD will be a material consideration in the determination of relevant planning application(s).



**Fig 5** Mill Road Depot Location Plan



## 2 SITE AND CONTEXT

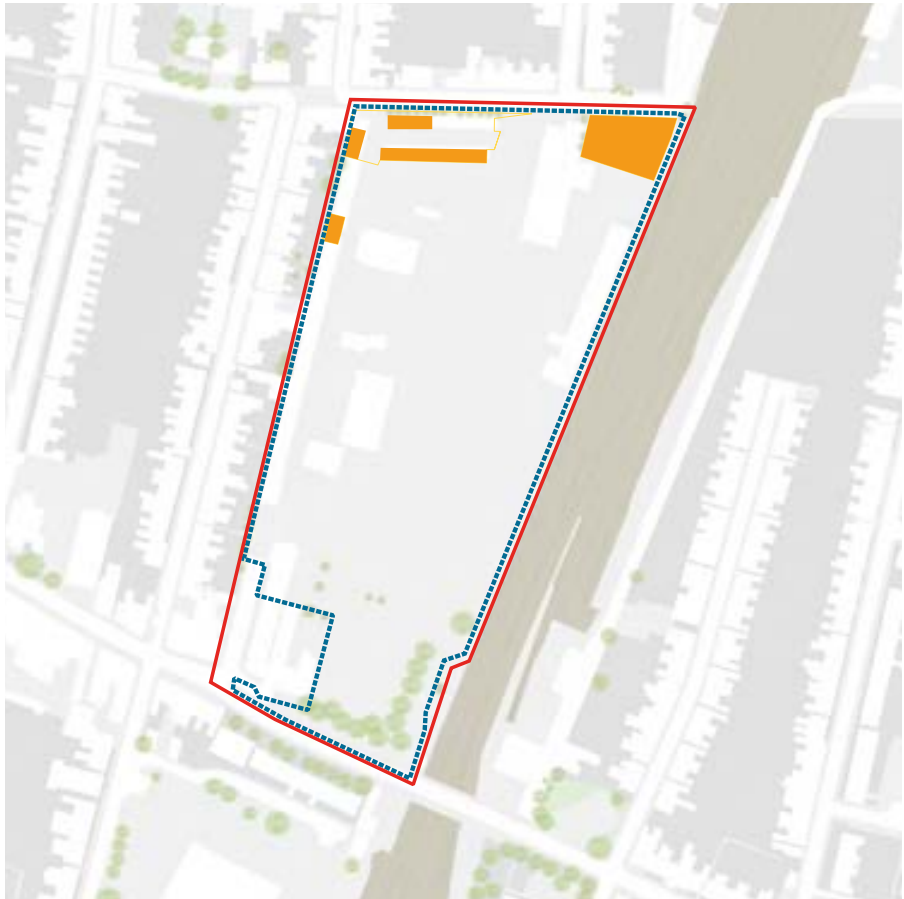
### 2.1 SITE LOCATION

2.1.1 Mill Road Depot is located off Mill Road, situated to the south-east of Cambridge city centre. It is immediately adjacent to the railway line to the east, and Kingston Street to the west (see figure 5). The site borders Hooper Street to the north and is immediately adjacent to Mill Road east and west local centres.

2.1.2 Figure 6 illustrates a selection of views of the depot site as it currently exists.



**Fig6** Existing site photographs. Above: Looking south towards the Mill Road entrance past the Gate House building and former Free Library. Below: Much of the site is in relatively low grade use with significant areas of hard standing.



- - - - - Council Freehold
- Current Leasehold
- Study area

**Fig 7** Cambridge City Council ownership



1880's



1920's



1960's

**Fig 8** Historic development



## 2.2 SITE DESCRIPTION

### Land ownership

- 2.2.1 The majority of the site is owned by Cambridge City Council but there are land holdings on the depot site which are occupied by the Regent Language School (see figure 7). Garages to the north-west of the site are the subject of long term leases. A lease ended in 2013 for The Wharf, situated to the north of the site, which is currently occupied by the Women's Resource Centre. The library building is owned by the County Council.

### Historic development

- 2.2.2 The site has always been the location of a number of industrial activities (see figure 8). In the 1880s, the site formed a Coprolite Mill, Iron Foundry and Timber Yard. These buildings, located centrally and across the eastern areas of the site, were removed in the 1900s, and a library constructed in the south western corner. The former library has now been converted into a community centre. A Coal Depot was constructed in the 1930s, along with warehouse buildings, which together occupied a large area of the site. The garages to the north-west were constructed in the 1980s. The site became the Council Depot in the 1990s, and more recently, a number of warehouse buildings in the centre of the site were demolished to form a waste segregation area within the depot.



**Fig 9** Site context and surroundings



**Fig 10** Existing site photographs. Above: The garages are situated at the northern end of the site adjacent to Hooper Street. Below: Women's Resource Centre

### Buildings on site and surroundings

- 2.2.3 A garage/servicing centre for council vehicles and a filling station are located at the northern end of the site. A number of commercial units are also located here which are let out to private companies. In the centre are a number of warehouses and a vehicle wash down area. The central eastern area of the site has until recently been used as a waste transfer station and is currently used for waste segregation and storage. Private garages are located to the north-eastern corner.
- 2.2.4 Coach houses along the western boundary are used as an office space for the City Council. These have the potential to be retained for adaptive re-use to residential accommodation. The southern area is predominantly used for parking, along with a canteen and additional council offices located within the south western corner in the Gatehouse building. The Free Library, to the south-west of the site, is a Grade II Listed Building and will need to be retained. The building used as a language school is situated to the south of the site and, as already noted, is in separate ownership.
- 2.2.5 The site is adjacent to a predominantly residential area of Petersfield comprised of two-storey terraced housing, although a large warehouse and yard is located to the south of a row of shops / flats on the south side of Mill Road. There are also a number of independent shops, cafes, bars and public houses that are situated on Mill Road.
- 2.2.6 Some of these key features are illustrated in figures 9 and 10.

-  Site entrance
-  Restricted site access
-  Main road way
-  Vehicular routes
-  No road access through
-  Planned junction improvement
-  Study area



**Fig 11** Vehicular access



## **Movement context**

### **Streets**







- 2.2.7 Key movement information is identified on figure 11.
- 2.2.8 The residential streets to the west of the depot site are narrow and can be busy routes. These are important vehicular and pedestrian routes linking Mill Road to Newmarket Road and the north part of Petersfield ward. Kingston Street has parking on one side of the road which makes it difficult for cars to pass in both directions. Pavement widths are also restricted. There is a road block which prevents vehicular access onto Kingston Street from Hooper Street, restricting vehicular movement from re-joining Mill Road.

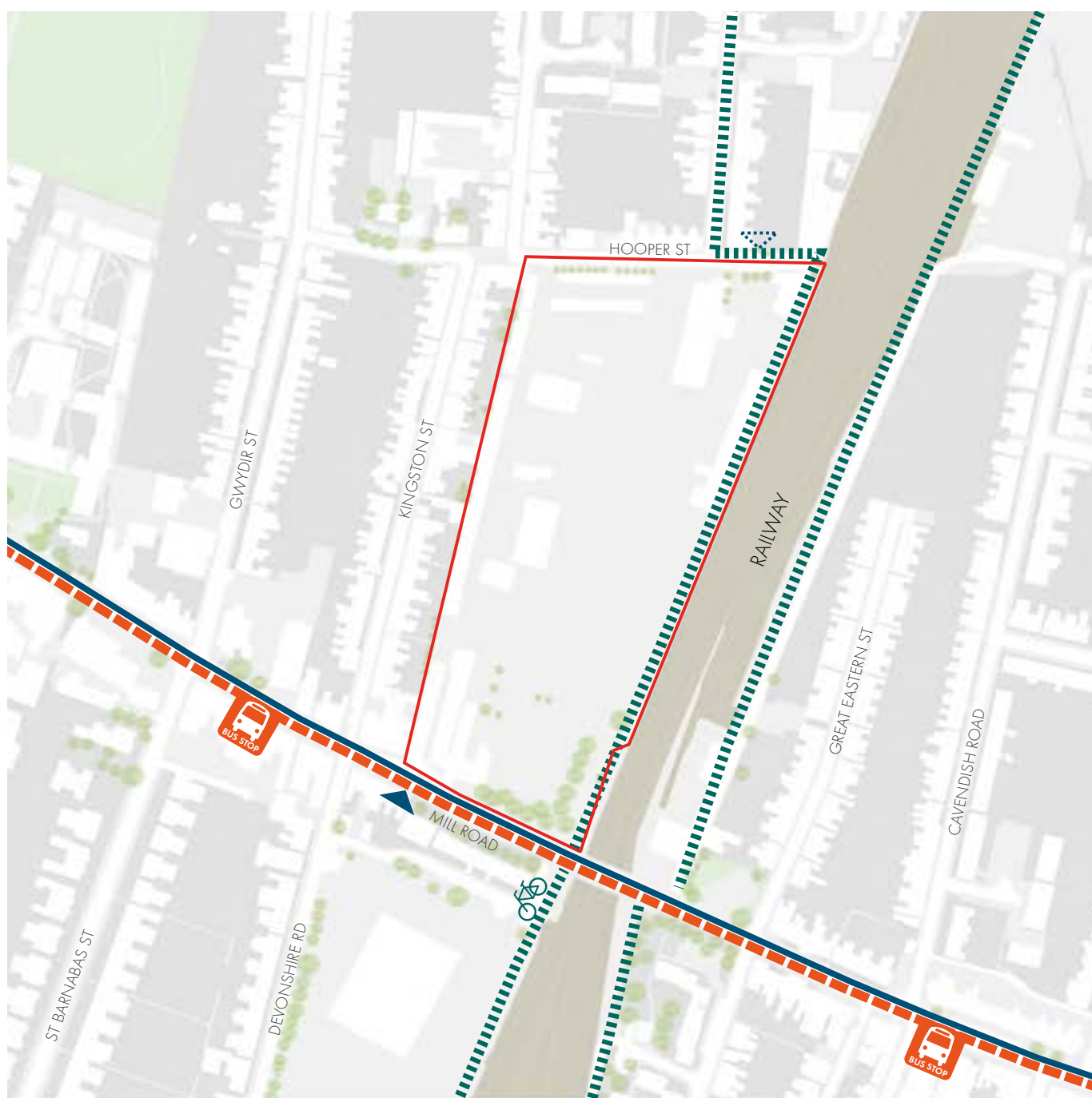
### **Parking**

- 2.2.9 The site is within a Controlled Parking Zone (CPZ) which means parking is restricted at certain times of day.

### **Rail and buses**

- 2.2.10 There is excellent accessibility to Cambridge Railway Station, a short 5 minute walk in a southerly direction from the site. Bus links are also good with the Citi 2 bus route connecting Mill Road with Cambridge City Centre and Addenbrookes. The Beehive Centre, a 13 minute walk away, is serviced by the 114, 17 and 196 bus routes, linking the centre to locations including Waterbeach and Newmarket.

-  Cyclist/pedestrian site entrance
-  Restricted site entrance
-  High Street and key pedestrian route
-  Bus route
-  Planned Chisholm trail
-  Study area



**Fig 12** Bus and cycle routes



### **Cycle and pedestrian network**

- 2.2.11 Mill Road is the main pedestrian route into Cambridge city centre. Footpaths are narrow, particularly on Mill Road Bridge, restricting pedestrian flow. On-road only cycling exists on Mill Road. Improvements to highway infrastructure may be necessary for the safety of cyclists and for improved accessibility to the site. There is a signed primary cycle network that crosses Mill Road from St. Barnabas Road to Gwydir Street, along Hooper Street and onto Ainsworth Street.
- 2.2.12 It is proposed that the Chisholm Trail will be a new north-south cycle route that will run along the railway line on the easterly side of the site. This will provide a high quality link to the station and to the planned Cambridge North station at its northerly end. It is currently proposed that the route will follow Ainsworth Street, and at Hooper Street, go diagonally across a section of the Women's Resource Centre space. At its southerly end, it will follow the boundary of the railway and pass under a side arch to give a safe link to Mill Road via the existing connection adjacent to the Free Library building. This path will need to be incorporated into any potential plans for the site.

- Public green space
- Play park
- Study area



**Fig 13** Local open space provision

### Open space

2.2.13 There is limited amount of protected open space in the area surrounding the Mill Road Depot site and the amount of informal open space in the ward is low given the very dense development form and the local population density. Figure 13 shows the amount of open space in the local area. Mill Road Cemetery (4ha) is the nearest protected natural green space, a 6 minute walk away. It is well used by the local community for recreation and is an important open space for wildlife. The other open spaces in the area are:

- St. Matthew's Piece – 0.6ha (11 min walk), playground facilities for young and old children.
- Romsey Recreation – 1.9ha (15 min walk), public park with a children's playground, green open space and mature trees.
- Coldhams Lane play area – 0.6ha (17 min walk)
- New Square Park – 0.75ha (16 min walk), green open space with planting.
- Parker's Piece – 9ha (9 min walk), green common near the town centre for recreational activities such as football and cricket.
- Parkside – 0.9ha (9 min walk)
- Coleridge Recreation - 4ha (10 min walk), football field, tennis court, paddling pool and a newly refurbished children's playground.

2.2.14 The development of the site is an opportunity to provide new open spaces for the local community.

- Employment
- Community (incl. education)
- Residential
- Mixed use
- Study area

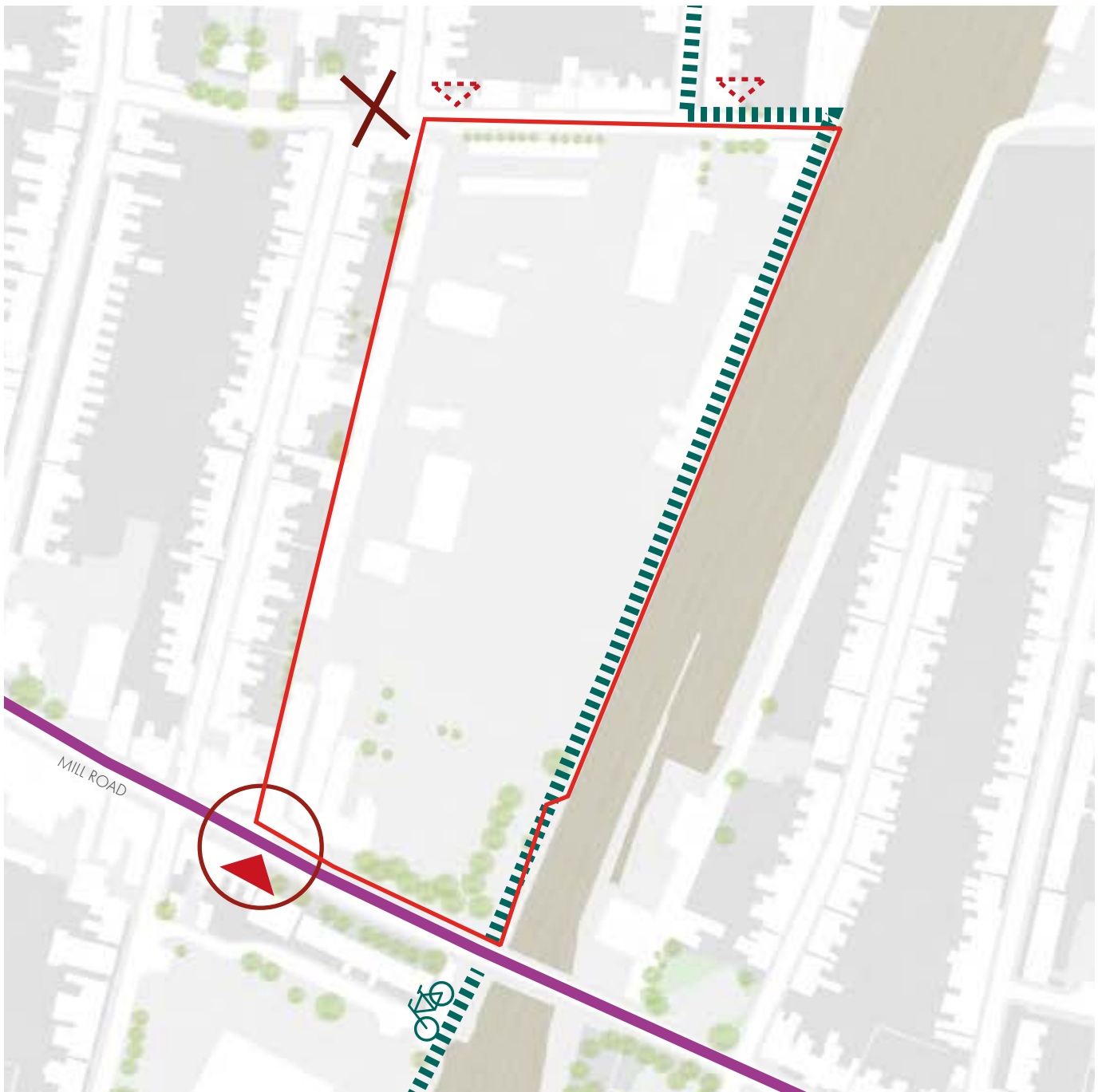


**Fig 14** Existing land uses

### Existing land uses

- 2.2.15 The majority of the site is currently used by the City Council as a Depot for many of its services, although some operations have recently re-located to a shared waste facility at Waterbeach with South Cambridgeshire District Council. There are two community buildings to the north-east and south-west of the site. These are currently occupied by the Women's Resource Centre and Indian Community and Culture Association. The Regent Language School is located to the south of the site. The leased garages are to the north-west of the site. Figure 14 shows the existing land uses on site.

-  Site entrance
-  Restricted site access
-  Main road way
-  No road access through
-  Junction improvement
-  Planned Chisholm Trail
-  Study area



**Fig 15** Site access



### Site access









- 2.2.16 Main access to the site is gained via a roadway leading off Mill Road, as shown in figure 15. Mill Road is a major vehicular route into and out of the city centre. The consultation from the Local Plan process raised concerns about the safety of the junction between Devonshire Road and Mill Road.
- 2.2.17 Mill Road is a very busy road and opportunities exist to improve access to the site. The junction currently suffers from poor visibility which is likely to be a constraint to be overcome as part of the scheme. Improvements to this junction will be necessary as part of any future re-development.
- 2.2.18 Vehicular access, both ingress and egress to the depot is restricted to the Mill Road entrance currently. The garages to the north-west off the site are accessed via Hooper Street.
- 2.2.19 The proposed Chisholm Trail will run through the site to the west of the railway line.

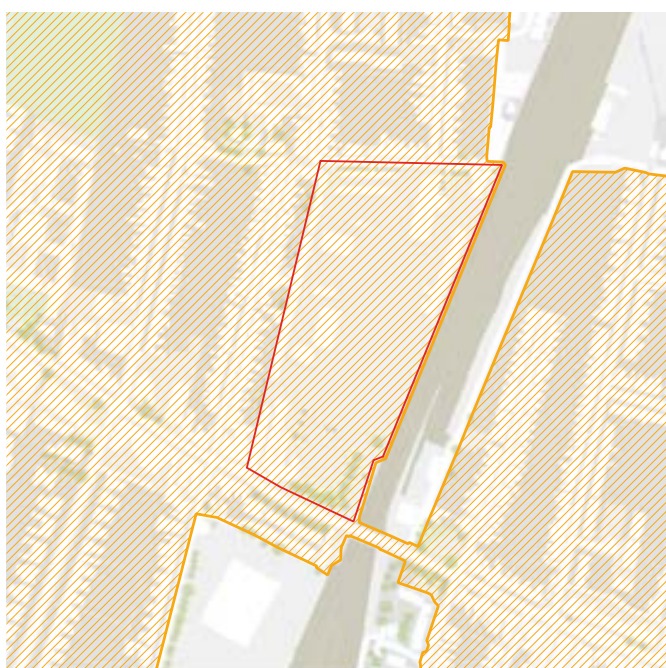


Fig 16 Existing building heights

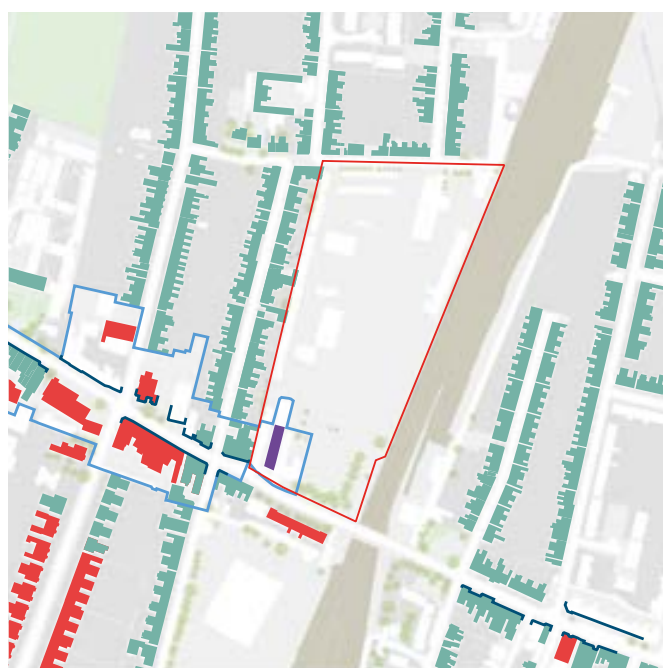
### Existing building heights

- 2.2.20 Building heights vary across the site, but predominantly comprise single-storey warehouses. The Free Library is also one-storey, although it is significantly taller and more striking than the one-storey warehouses. The servicing centre, Gatehouse building and the offices to the west are two-storey buildings. The Regent Language School, close to Mill Road to the south of the site, is a three-storey building.
- 2.2.21 The area surrounding the site is mainly comprised of residential two-storey housing, although there are a number of three-storey houses on Devonshire Road and St. Barnabas Road, to the other side of Mill Road. A few four-storey buildings form an exception to the average building heights in the area, and these tend to mark a specific use (e.g. churches and the brewery). Existing building heights are shown in figure 16.

-  Conservation Area
-  Grade II Listed building
-  Buildings of local interest (as defined in CA appraisal)
-  'Positive' impact (as defined in CA appraisal)
-  'Negative' impact (as identified by consultant team, 2015)
-  Historic High Street frontage
-  Neighbourhood area
-  Study area



**Fig 17** Conservation area boundary



**Fig 19** Townscape analysis



**Fig 18** Listed buildings and buildings of local interest



**Fig 20** Buildings of less historic merit

## Heritage assets

### Conservation Area

- 2.2.22 Mill Road Depot is located within the Mill Road Conservation Area, as defined in the Mill Road Conservation Area Appraisal (2011) prepared by Cambridge City Council and shown in figure 17. The area forms part of the 'Cambridge Conservation Area No. 1 – Central' which was originally designated in 1969, with the addition of Mill Road and St. Matthew's area in 1993.
- 2.2.23 The main characteristic of the area are two-storey terraced houses, which directly address the pavement, and form long lines of continuous frontages. The streets are set out in a grid pattern at right angles to Mill Road. Another feature of the area are the preserved and well-detailed Victorian buildings surrounding Mill Road, with only a few examples of modern infill.
- 2.2.24 The area is home to a diverse multi-cultural community. It includes a number of mid to late 19th century buildings forming a mix of commercial, residential, religious and community uses. Mill Road itself has a number of two and three storey historic buildings with continuous frontages, contributing to the distinct character of the area. Some of these shopfronts have undergone replacement in recent years.
- 2.2.25 Under the Planning (Listed Buildings and Conservation Areas) Act 1990, the area is considered desirable to preserve or enhance for its 'special architectural or historic interest'. Early consideration would need to be given to the impact of proposals on the setting and character of the Mill Road area for the redevelopment of the Mill Road Depot.
- 2.2.26 The main issues for the Mill Road Conservation Area, as identified in the Conversation Area Appraisal and of relevance to the depot site are:
- Preserving and enhancing the unique character of the Mill Road shopping area – improvements need to be made to some of the buildings and poor quality shopfronts.
  - Public realm proposals – improvements need to be made to the poor quality public realm on Mill Road, including lighting, protection of historic street name plates, retention of historic street signs and pavement improvements.

### Listed Buildings

- 2.2.27 A Grade II Listed red brick and terracotta neo-Baroque building, constructed in 1897, is located in the south-western area of the site (see figure 18). Prior to its use as an Indian Cultural Centre, it was a library and was first listed in 1972. The building is of special architectural and historic interest and should be retained as part of any future re-development of the site. Consent must be sought to carry out alterations that would affect its character and setting.
- 2.2.28 There are three other listed buildings in the Conservation Area – St. Matthew's Church, the Cemetery Lodge and Hughes Hall which are illustrated in the Conservation Area Appraisal (2011).

### Buildings of Local Interest

- 2.2.29 There are a number of buildings in the area surrounding the depot site that are of local interest (see figure 18). These typically are terraced or semi-detached houses which vary in size and detail, but there are also a number of buildings in non-residential use, including a school, community buildings, several former warehouses, other industrial buildings, and a group of purpose-built shops in Mill Road. Buildings of Local Interest do not have the same statutory protection as graded buildings but are nevertheless considered heritage assets.





**Fig 21** Photographs of the conservation area including the Listed former Free Library building on the depot site (top four photographs) and surrounding streets



### **Positive and negative buildings**

- 2.2.30 The Mill Road Conservation Area Appraisal (2011) gives details of a number of positive buildings of townscape merit, and shown in figure 19. These are buildings that have been relatively unaltered where their style, detailing and building materials provides the streetscape with interest and variety. Crucially, they make a positive contribution to the special interest of the Conservation Area. Most of the positive buildings in the area are mid to late 19th century terraced houses, and are mainly one bay wide.
- 2.2.31 There are a number of buildings on site that the Council has identified as negative buildings that have relatively less historic or design merit than other buildings in the area. These are shown in figure 20. These are not identified in the Area's Conservation Appraisal document but, similar to the buildings identified in the Appraisal, make a negative contribution to the Mill Road Conservation Area. It is therefore deemed appropriate to remove these and replace them with buildings that will enhance the character and setting of the Mill Road conservation area.
- 2.2.32 A selection of existing photographs of the Conservation Area are provided in figure 21.

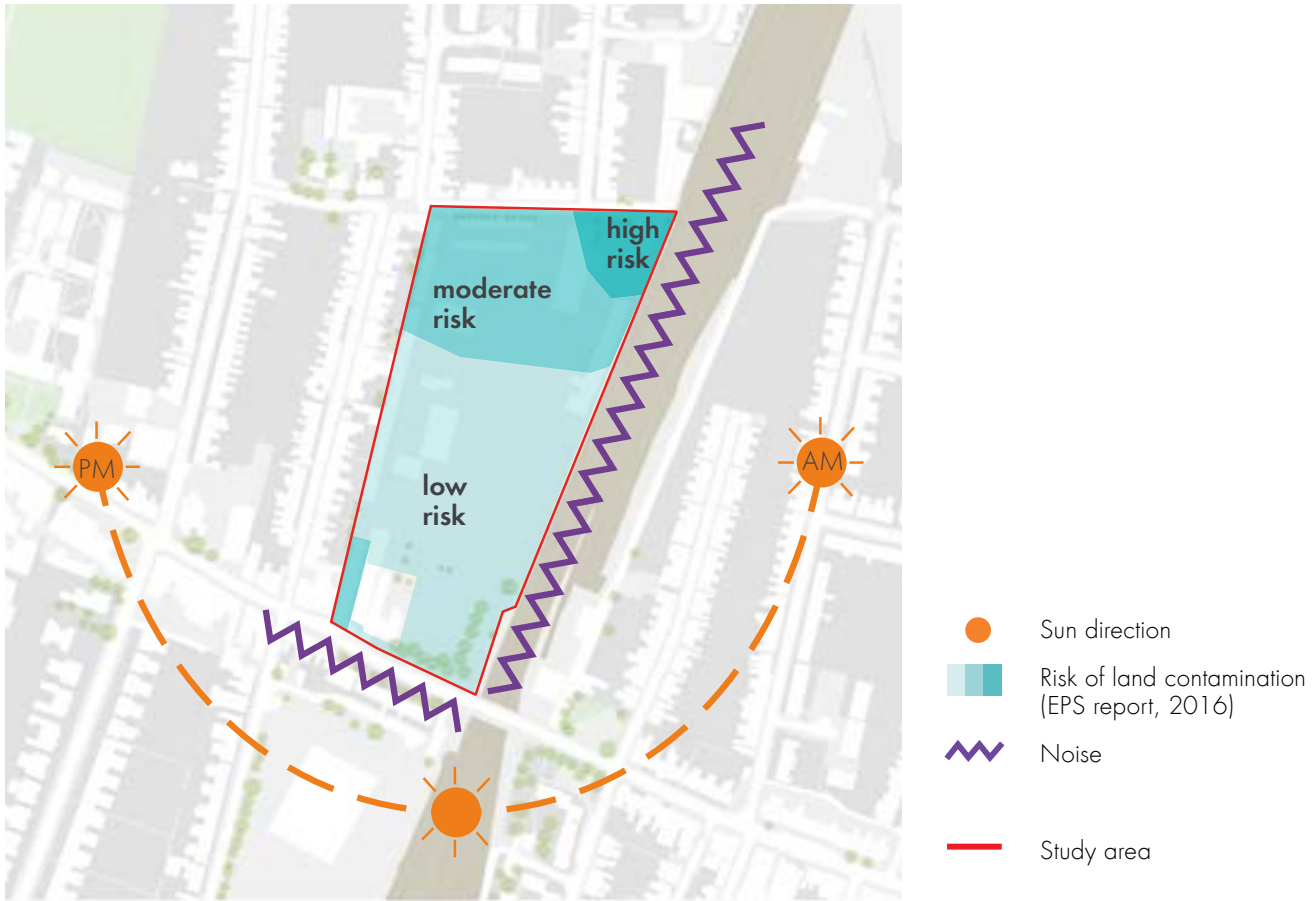
- Private garden
- Hard standing and pavement
- Public
- Informal/fringe landscape
- Car parking
- Important trees (as identified in Conservation area appraisal)
- Prominent lime trees
- Study area



**Fig 22** Landscape assets and analysis

### **Trees and landscape**

- 2.2.33 Mature trees make a significant contribution to the appearance and character of the Conservation Area. There is a row of mature trees to the north of the site to the south side of Hooper Street that are important to the streetscape and must be protected and enhanced under the Area's Conservation Appraisal document. The trees at the southern end of the site adjacent to the bridge and language school are also important.
- 2.2.34 Due to the use of the site for industrial purposes, there is a deficiency in landscape features across the site. A cluster of mature trees are located to the south-east of the site which constitute a small amount of informal landscaping and are worthy of retention. Figure 22 illustrates existing landscape assets and analysis.



**Fig 24** Environmental considerations



**Fig 25** Drainage [Cambridge City Council, January 2016]

## Environmental considerations

### Noise

- 2.2.35 Key environmental considerations are identified on figure 24.
- 2.2.36 A noise assessment will be required to identify the potential impact of noise levels from the railway on the amenity of new residents of the Mill Road depot site. Attenuation measures may be necessary and innovative design solutions sought. A number of measures can be introduced to control the source of, or limit exposure to, noise and vibration as necessary.

### Land contamination

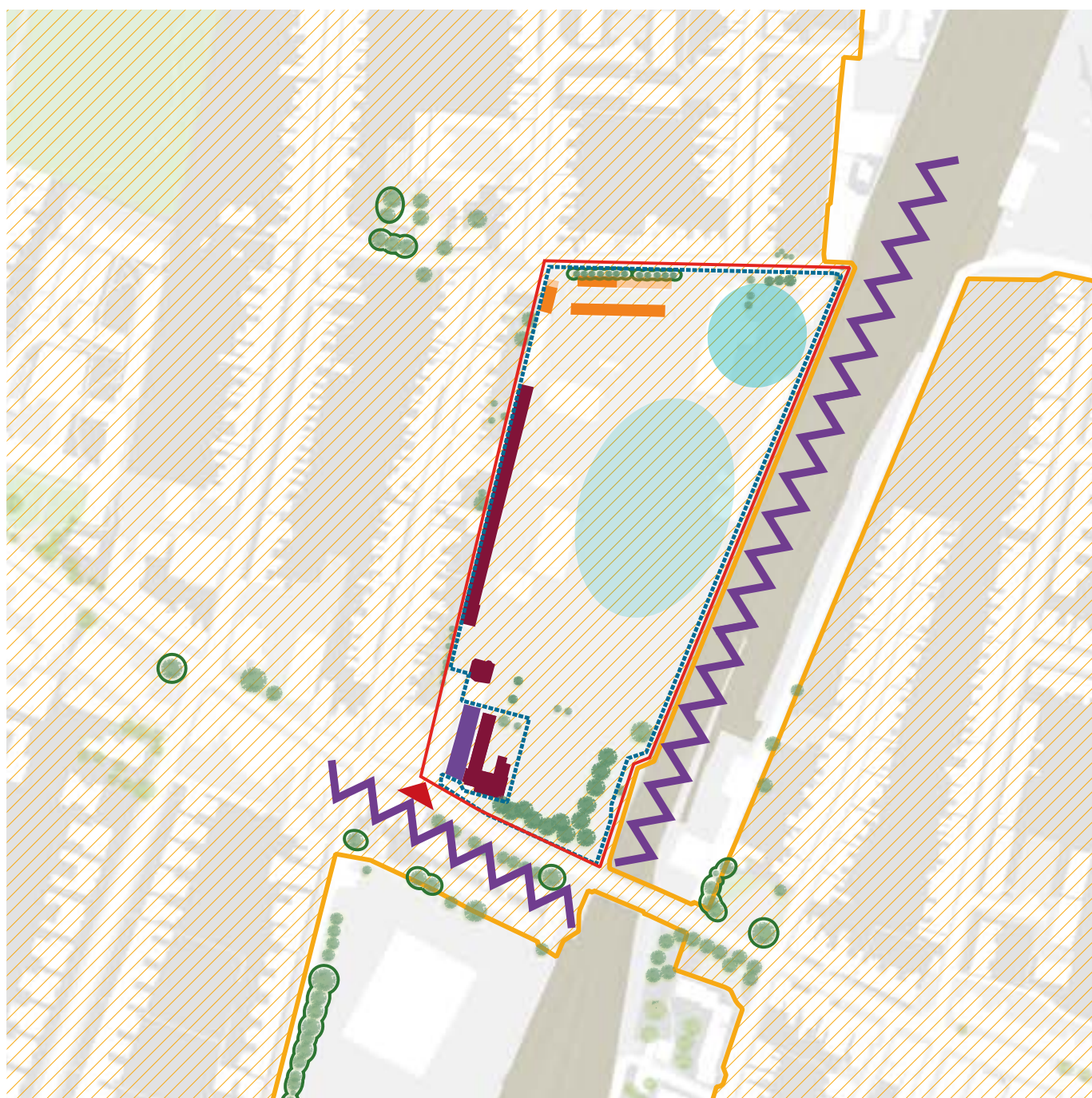
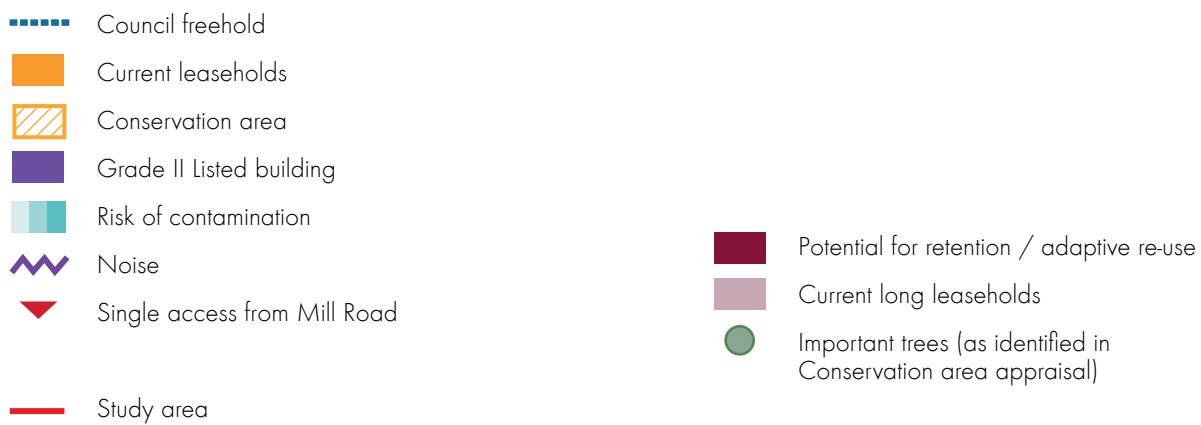
- 2.2.37 Given its present and historical industrial uses, there are pockets of contamination on-site. Cambridge City Council commissioned a survey in 2015 to identify the extent and impact of this on future development. The findings showed a general north-south split in terms of contamination across the site.
- 2.2.38 Elevated concentrations of various contaminants were identified associated with made ground across the site, which would not be suitable for use within proposed domestic garden or landscaped areas. More localised contamination was identified within the northern part of the site, especially surrounding the underground storage tanks (USTs). Impacts to soil and groundwater from fuels were identified within this area which will require further assessment.

### Drainage and flooding

- 2.2.39 The site generally falls towards Hooper Street with levels around 15.0m A.O.D. adjacent to Hooper Street and levels of around 16.4m A.O.D. in the south east corner and 15.8m A.O.D. outside of the gatehouse.

- 2.2.40 Current surface water drainage discharge is located in Hooper Street and in Mill Road. The surface water sewer in Hooper Street is 1.3m deep and the surface water sewer in Mill Road is 1.1m deep. Both of these are shallow and will mean that any drainage will have to be as high as possible to allow discharge under gravity.
- 2.2.41 The site lies within Flood Zone 1, defined by the National Planning Policy Framework (NPPF), and therefore has the lowest potential risk of fluvial or tidal flooding. There are minor surface water issues that can be mitigated against through good design. Opportunities exist to introduce sustainable urban drainage systems such as ditches, swales and ponds as part of future proposals.
- 2.2.42 Current foul drainage and surface water drainage is shown indicatively in figure 25.





**Fig 26** Summary of constraints



## 2.3 SUMMARY OF OPPORTUNITIES AND CONSTRAINTS

2.3.1 This chapter has highlighted a number of key constraints and opportunities that should be taken into consideration for future development of the Mill Road Depot site. These have been summarised below and are shown in figure 26.

### Constraints

2.3.2 The site has the following key constraints:

- The Language School site and buildings are not in City Council ownership;
- Vehicular access only from Mill Road and vehicular movement currently prevented re-joining Mill Road from Hooper Street;
- Poor cycling provision along Mill Road;
- The site is near a dangerous junction;
- Grade II listed library should be retained;
- The site lies within a Conservation Area;
- Potential impact of sound pollution from the railway on development; and
- Ground contamination is likely to require remediation and mitigation.

### Opportunities

2.3.3 The opportunities of the site are:

- Integrate existing landscape and trees.
- Adapt and integrate Grade II Listed Building and coach house building on west side of site.
- Integrate existing garages in short term in such a way as to enable future housing fronting Hooper Street in the long-term.
- Re-integrate existing / invite new community uses to populate existing / new buildings.
- Create new edge to railway line.

## 3 VISION AND KEY PRINCIPLES

### 3.1 VISION

“Mill Road Depot site will become a popular residential neighbourhood, creating a network of pedestrian, cycle routes and open spaces which connect the site to the surrounding Petersfield neighbourhood, and key destinations elsewhere in the city. The development will establish a varied mix of approximately 167 new dwellings including a significant proportion of affordable homes.

The design will respect the typical form, scale and character of buildings and streets in the Mill Road Conservation Area, exploiting opportunities to incorporate taller buildings up to four storeys adjacent to the railway at the eastern boundary of the site. The proposals will respond to the sites’ key opportunities and constraints, including the provision of a single point of general vehicular access from Mill Road alongside a low level of car parking provision.”

## 3.2 PRINCIPLES

### Access and permeability

- 3.2.1 The proposals will establish a single point of vehicular access from Mill Road via the existing entrance to the site. Subject to more detailed design work and assessment, this will be facilitated by improvements to the junction, a low level of on-site parking and an integrated network of pedestrian and cycle connections across the site, including the Chisholm Trail which runs through the site. Secondary emergency vehicle and pedestrian and cycle only access will be provided from Hooper Street to the north.

### Housing-led mix of uses

- 3.2.2 Housing is the principal land use for the Depot site and will include a significant proportion of affordable housing. A range of different affordable delivery models will be considered, with an overall ambition of 40% affordable housing as a minimum target. Community uses could also be provided.

### Design

- 3.2.3 Proposals should provide a contextual approach to scale and massing in response to the typical domestic scale of development on surrounding streets and the less sensitive edge presented by the railway corridor to the east of the site. The proposals should retain the Listed former Free Library building, the Language school and the gatehouse building towards the south of the site. There is also an opportunity to keep and refurbish the coach houses on the western edge of the site. The block of garages to the north should be retained in the short-term but any scheme should be capable of accommodating their phased, future redevelopment. Development should be arranged around a dominant north-south grain of streets and spaces. Opportunities for integrated, sustainable design solutions should also be pursued.

### Open spaces / environment

- 3.2.4 The proposals should embed a positive approach to open space in response to existing and future need. Open space should be viewed as a multifunctional resource which can accommodate a wide range of activities, playspace and environmental features including surface water attenuation, ecological enhancement and landscape proposals as appropriate. Several spaces should be provided of varying size and function. Key spaces include a new neighbourhood park to the north and a flexible space adjacent to the Chisholm Trail and Mill Road to the south-east.

**KEY PARAMETER DIAGRAMS AND GUIDANCE:**

Key principles and guiding statements to inform planning decisions and design development are provided as follows.

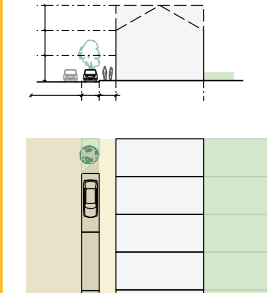


**ILLUSTRATIVE MATERIAL:**

Examples of how the parameter diagrams and guiding principles could inform future proposals. These illustrations are indicative and do not have specific status or weight in planning terms.



**Indicative parking typologies (section 4.7)**



**Indicative street sections (section 4.7)**



**Fig 27** Summary of guidance in chapter 4 highlighting the key parameter plans for guidance purposes, and supporting illustrative material.

## 4 DEVELOPMENT PARAMETERS

### 4.1 OVERVIEW

- 4.1.1 This chapter provides planning and design guidance on how the development principles will be used to guide future planning applications. In some cases the wording is more prescriptive, and this is reflected in the language with words such as “will” and “should”. In other cases the guidance is more discretionary and illustrative, providing a vision and aspirations for future development.
- 4.1.2 Figure 27 summarises the broad scope and status of the material. The main guiding principles are defined in a series of “parameter” drawings with supporting text. These are supported by a range of illustrative drawings which depict how the principles could be realised.
- 4.1.3 As well as complying with the planning and design guidance in this SPD, any future planning applications should comply with the policies in the Cambridge Local Plan (2006) and its replacement currently the subject of an examination led by a government inspector. This chapter is structured under the following headings:
- Site layout
  - Transport and access
  - Open space
  - Uses
  - Building heights
  - Character and form
  - Environmental considerations and sustainability
  - Planning obligations










- ▼ Vehicular access
- ▼ Cycle and pedestrian access only
- Primary vehicular routes
- - - Secondary vehicular routes
- ⋯ Cycle and pedestrian links only
- 🚲 Planned Chisholm trail
- Potential for retained buildings
- Green space
- Low/medium density housing
- Medium/high density housing
- Community / mixed use
- Study area
- └─┘ Primary frontages

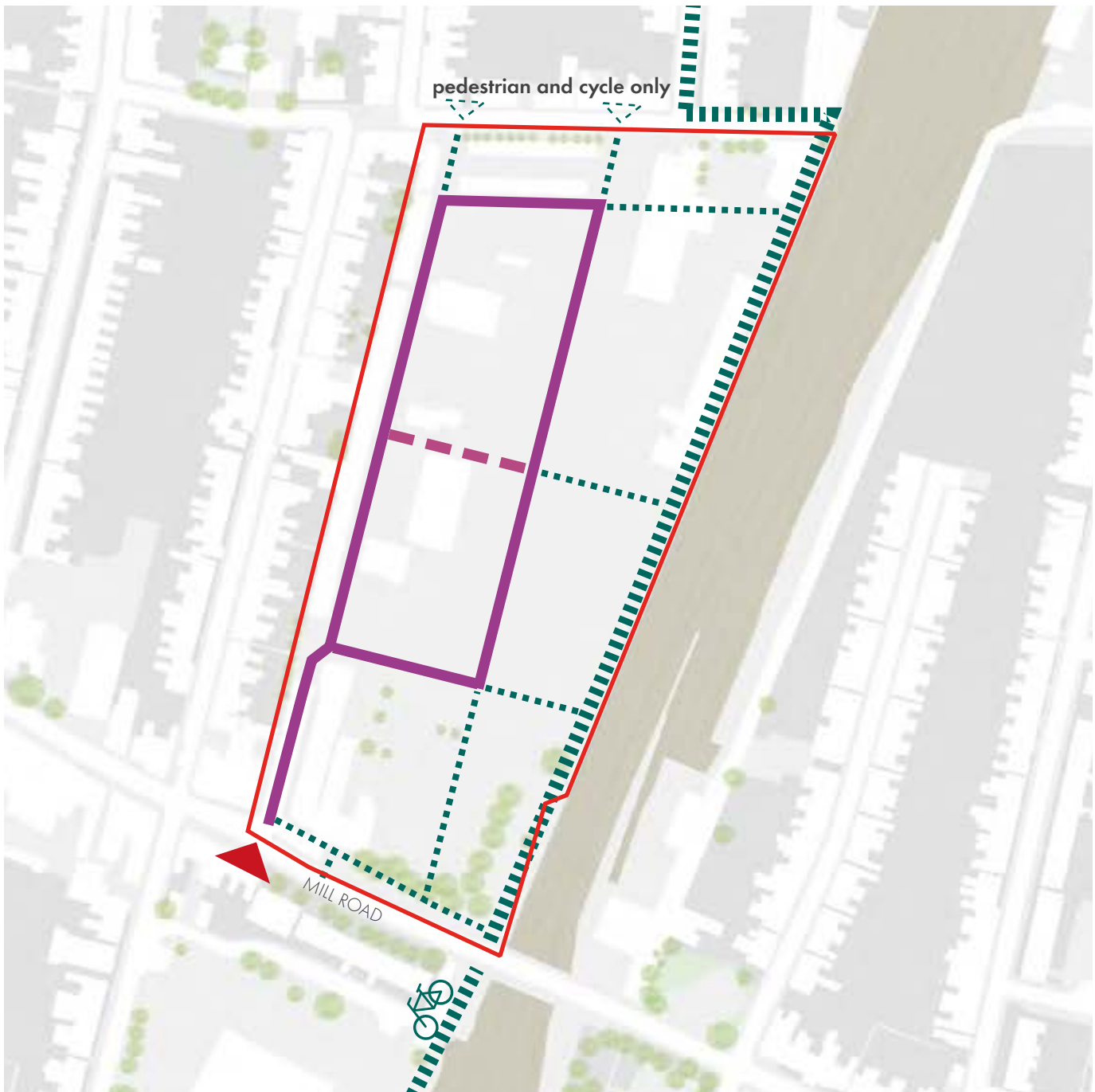


**Fig 28** Site Framework plan

## 4.2 SITE LAYOUT

- 4.2.1 The site layout (see figure 28) assumes the retention of the existing access route running north-south through the site from the Depot entrance at Mill Road towards the garages to the north. This alignment is informed by the retention of the Grade II Listed Building and undesignated Gateway building at the entrance to the site.
- 4.2.2 The primary route will be complemented by a new central route situated broadly in parallel and to the east. The precise position of the new route will be determined by detailed design work including the dimension of the development block / open spaces between the primary routes.
- 4.2.3 East-west linkages should be established to connect the primary north-south routes. These will be informed by the precise position of residential frontage and open spaces.
- 4.2.4 Tertiary pedestrian and cycle links will complement the dominant north-south grain and the secondary east-west linkages. Of particular importance is the Chisholm Trail which coincides with the eastern boundary to the site adjacent to the railway.
- 4.2.5 Key frontages have been highlighted on the drawing. The precise location of these will evolve through the detailed design process at a planning application stage, but it is important that frontages define key streets and spaces as illustrated in the drawing.
- 4.2.6 Reference should also be made to the following sections on transport and access (4.3), open space (4.4) and character (4.7).

-  Site entrance
-  Restricted site access
-  Primary vehicular routes
-  Secondary vehicular routes
-  Links to Chisholm trail
-  Planned Chisholm trail
-  Study area



**Fig 29** Transport and Access

## 4.3 TRANSPORT AND ACCESS

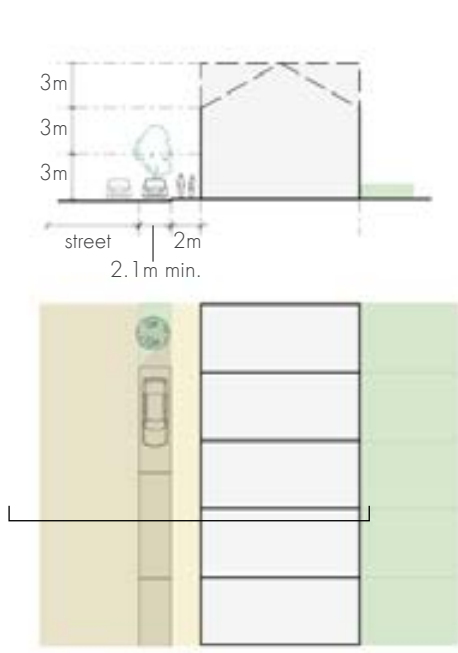
- 4.3.1 The overall approach to transport and access is summarised on Figure 29.

### **Pedestrian and cycle connectivity**

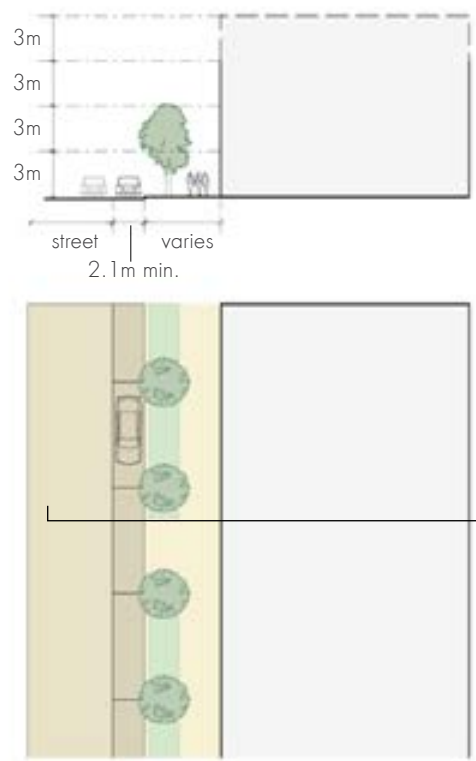
- 4.3.2 Proposals should make full provision for the delivery of the Chisholm Trail cycle route which will enter the site from the south via the arch formed by the railway bridge at the southern boundary of the site. The route should be incorporated within the demise of Depot site ownership, running parallel to the railway line and reconnecting to the on-street network at Hooper Street. The Chisholm Trail proposals (see figure 29) assume a parallel north-south route to the east of the railway line. Proposals should demonstrate how wider cycling patterns have been considered to ensure an integrated approach in the local area.
- 4.3.3 For the purposes of the SPD, it is assumed that the Chisholm Trail would be accommodated as an off-road, segregated route with a minimum of 6m width, or otherwise identified in guidance on the Chisholm Trail. Opportunities to incorporate other objectives such as biodiversity mitigation, drainage or landscape planting should be pursued.
- 4.3.4 Proposals will be required to demonstrate an appropriate cycling strategy in terms of the immediate site, and wider cycling journeys which interface with the site. Particular attention will be required to illustrate how cyclists heading east from the City Centre join the Chisholm Trail. There is a natural desire line for cyclists to enter the site of the Mill Road Depot junction and join the Chisholm Trail (heading north of south) via the path in front of the language school. This movement requires careful design review in the context of potential junction enhancements (see below) to ensure a safe, formalised means of access. Although northward movements will be possible, the County Council might seek to encourage principal strategic cycle journeys via the branch of the Chisholm Trail to the east of the railway bridge.
- 4.3.5 Sightlines between local cycle routes and the main Chisholm Trail will need to be reviewed to encourage safe access.
- 4.3.6 Local cycle and pedestrian only connections will be encouraged on-site. Pedestrian and cycle connections will also be accommodated on primary and secondary routes. Cycle, pedestrian and emergency only movements will be accommodated at the points of connection to Hooper Street.

### **Vehicle access, routes and hierarchy**

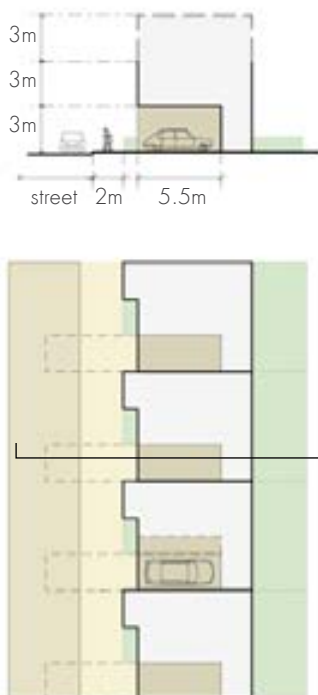
- 4.3.7 In line with the site allocation, vehicle access to the site should be from the Depot entrance on Mill Road.
- 4.3.8 Cambridge City Council is reviewing the primary access point which will consider the junction layout in the context of traffic count analysis, a Stage 1 Road Safety Audit (RSA1), the general layout of the site and emergency access requirements. As part of this, the City Council is working closely with Cambridgeshire County Council to consider basic requirements for safe ingress and egress to the site. A more detailed junction concept and assessment will be required as part of any future planning application. As one solution, it is likely that the existing bridge ramp, pier and railing at its north-westerly corner will need to be widened out to improve sightlines when exiting the site.
- 4.3.9 Access to leased garages only or emergency vehicles only is likely to be provided from the northern end of the site from Hooper Street. Subject to more detailed design, this could



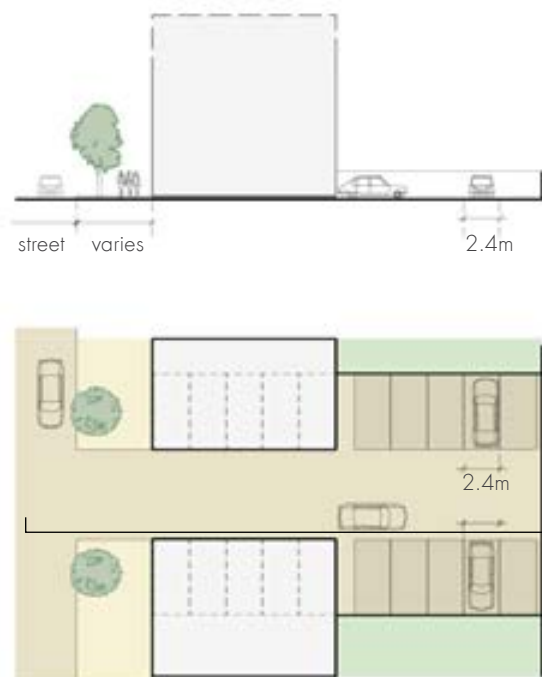
**Fig 30** Parking typology - on-street



**Fig 32** Parking typology - on-street with buffer



**Fig 31** Parking typology - garage / undercroft



**Fig 33** Parking typology - parking yard or at grade deck parking



be achieved via the existing entrance to the garages, or from a new point of entrance on alignment.

- 4.3.10 Any future site developer will need to demonstrate appropriate capacity at the Mill Road junction for all vehicles to/from the site in addition to required works to facilitate these movements.
- 4.3.11 Developers will be encouraged to incorporate a traffic calmed environment using street design and intersecting cross routes to create a natural reduction in speeds. Shared surface environments should be employed including for the retained route adjacent to the coach houses at the western edge of the site.
- 4.3.12 The presumption is that existing access restrictions on surrounding streets would remain.
- 4.3.13 Cambridge City Council's car parking standards are expressed as maximum standards in line with national guidance and the City's Council's sustainability aims. The Local Plan identifies a justification for car parking provision below this maximum given the site's close proximity to public transport, shops and services. Based on discussions to date, the aspiration is to provide low car parking provision with an additional visitor parking space of 0.25 per home (i.e. 1 visitor space for every four homes). Any parking allocation will need to be justified in future planning applications in the context of more detailed design, assessments and the exact proportion of house types.
- 4.3.14 Car parking should be provided in a range of typologies (see figure 30 to 33), including parallel on-street bays, on-plot in integrated garages or undercroft spaces or allocated off-street parking courts / at grade "decks". The potential for the use of car clubs and electric vehicle charging points within the development as a whole should be investigated early in the development process.
- 4.3.15 The Council will encourage early liaison with Cambridgeshire County Council to consider the wider strategy for the adoption of streets and car parking management including potential for a pay and display regime.

### **Car parking**

- 4.3.13 Cambridge City Council's car parking standards are expressed as maximum standards in line with national guidance and the City's Council's sustainability aims. The Local Plan identifies a justification for car parking provision below this maximum given the site's close proximity to public transport, shops and services. Based on discussions to date, the aspiration is to provide low car parking provision with an additional visitor parking space of 0.25 per home (i.e. 1 visitor space for every four homes). Any parking allocation will need to be justified in future planning applications in the context of more detailed design, assessments and the exact proportion of house types.



**Fig 34** Open space

## 4.4 OPEN SPACE

- 4.4.1 A significant proportion of the Mill Road Depot site should be allocated to open space as set out in figure 34. Through consultation, stakeholders identified an aspiration to create a balanced approach to open space which combines the needs of the wider Mill Road community, the existing neighbourhood to the north and new residents on the site.
- 4.4.2 In this context, the open space strategy focused on two primary spaces; a new space adjacent to Mill Road bordered by the Chisholm Trail as it enters the site and an open space to the north in closer proximity to Hooper Street. This spatial strategy supports initial guidance / advice relating to sustainable urban drainage (SUDS) which is likely to necessitate a minimum space allocation for on-site attenuation requirements in close proximity to existing infrastructure at the north and south of the site. Further guidance is set out in section 4.8.
- 4.4.3 Subject to detailed design process including the delivery of the full housing allocation and adherence to the key principles and parameters in this chapter, it is anticipated that a future scheme could indicatively accommodate approximately 20% to 25% of site area as open space.
- 4.4.5 Street trees should be planted on the primary north-south streets, the Chisholm Trail and to mark the boundaries of key open spaces. Species should be selected to establish a sense of hierarchy through the streets and spaces including larger trees on key routes, junctions and spaces. Opportunities to establish early planting regimes will be encouraged alongside other key elements of site infrastructure to accelerate place-making benefits for new streets and urban spaces on the Depot site.
- 4.4.6 New development schemes should take a proactive approach in consulting with the Conservation team and Tree Officers to review the strategy to retention and selective removal / replacement. For example, the existing mature trees at the south eastern corner of the site form a distinctive boundary to the site which might require active stewardship and management through appropriate surveys and interventions.

### Open space character areas

- Street trees**
- 4.4.4 Existing trees are an important factor on development sites and a material consideration in the UK planning system. There are trees located on and immediately adjacent to the Depot that will need to be considered as part of any redevelopment proposals. In accordance with BS5837: 2012 Trees in relation to design, demolition and construction - Recommendations, the above and below ground tree constraints should be collated by an arboriculturalist and this information used to inform development layouts.
- 4.4.7 As set out above, any future planning applications should set out a comprehensive open space strategy which articulates a distinct approach to formal and informal spaces across this accessible urban site. Although there is a degree of flexibility, any scheme should seek to include the following elements. Historic site plans have inspired the identification of possible names for the key spaces, but these will be subject to further discussion and review as the project moves forward.
- “The Limes”: Proposals should seek to embed a new space at the south eastern corner of the site as a cornerstone of the scheme. This space will provide a flexible asset for the Mill Road area being within easy walking distance of the principal pedestrian entrance to the site and immediately adjacent to the Chisholm Trail as it enters the site from the south. This space, bounded by mature trees, will be defined as



**Fig35** Accordia - positive example of landscaping



**Fig36** Hampden Gardens - sculptural play space and landscape treatment



**Fig37** Play area at Jesus Green



**Fig38** Pym Court - varied public realm and landscape treatment



an attractive formal setting with potential for a wide range of regular, or more temporary, events which appeal to new residents and the wider neighbourhood. As noted above, some interventions might be required subject to more detailed assessment of existing tree health. Replacement species should achieve a similar scale and enclosure to this important edge. As noted in section 4.8, it is likely that this space would form part of a wider drainage strategy accommodating water storage (attenuation) for the purposes of managed discharge to the south of the site.

- “Mill Park”: Future development should also include a new more central local park with appropriate playspace and facilities to serve the new community and existing residents in the wider neighbourhood. This space has potential to include larger species trees to create an attractive focal point at the heart of the development. As set out in section 4.8, it is likely that provision for surface water attenuation will be required as part of Mill Park to accommodate discharge to the northern part of the site.
- “Eagle Foundry Walk”: A linear landscape feature could be incorporated adjacent to the central north-south street, forming a generous formal edge to the more substantial apartment buildings which form a strong urban boundary adjacent to the Chisholm Trail and the railway line. The exact layout and orientation of this space in relation to the residential frontages should be resolved at a detailed design stage. For the purposes of the illustrative scheme, the apartments and associated linear space deviates from the dominant alignment of the primary north-south streets in response to the orientation of the railway line and Chisholm Trail – widening from south to north and then cutting back to form an axial connection to Ainsworth Street. This is likely to form a popular pedestrian route through the site and will also form an edge to a less formal on-street cycle connection which complements the Chisholm Trail. The space offers opportunities for street tree planting and incorporation of biodiversity measures.
- Chisholm Trail: A 6m zone should be reserved at the eastern edge of the site to accommodate the segregated, off-road Chisholm Trail. This linear route will make allowance for a 2-way cycle way and a separated cycle zone alongside appropriate public realm and planting. The route could form part of a wider green infrastructure strategy with biodiversity and drainage functions. Early liaison with relevant officers at the City Council and County Council will be required to ensure integration with wider design proposals for this city-wide cycling strategy. Careful consideration of patterns of cycle and pedestrian movement across the wider Depot site will be required to mitigate potential conflicts and safety issues.
- “Gatehouse courtyard”: The illustrative scheme highlights the potential for a new local square adjacent to the retained and refurbished Gatehouse building. Depending on the exact layout and mix / disposition of uses, this could have a community focus, or take on a semi-public character with connections to adjacent community uses including a nursery.

### Play space

- 4.4.8 Children’s play space for a range of ages should be provided in the development. It is estimated that approximately 0.13ha of equipped play space / outdoor provision would be required on-site. Allotments and opportunities for communal growing could be provided on-site, depending on local demand. Examples of spaces are illustrated in figure 35 to figure 38.

### Outdoor sports pitches

- 4.4.9 Given the relatively small size of the site and its long rectangular form, it is not appropriate to provide outdoor sports pitches on site. Commuted sums for off-site provision or improvements in lieu of on-site provision will be sought.

### Shadow studies

- 4.4.10 All proposals for the site should use shadow studies at different times of the year to demonstrate that the public open space receives a reasonable amount of sunlight.



- Mixed use (e.g. community and residential) - flexibility to provide community uses elsewhere on the site subject to design development
- Residential
- Study area



**Fig 39** Indicative uses

## 4.5 USES

### Housing

4.5.1 In line with the site allocation, the primary land use will be housing. The site has capacity for 167 homes during the Local Plan period to 2031. The indicative approach to uses is set out on figure 39.

4.5.2 The site is suitable for a range of house typologies and it is anticipated that the City Council will seek to achieve a balanced mix of housing and apartment typologies. Based on initial capacity studies, the 167 home allocation is likely to be apportioned as 50% to 60% apartments and 40% to 50% houses. The exact mix is flexible and will be informed by a consideration of housing market dynamics, approach to tenure, and disposition of open space.

4.5.3 The images overleaf (figure 40) illustrate a range of local typologies, traditional and contemporary in form, which could be appropriate on the site. These include terraces, town houses and mews as well as apartments or mansion blocks. The precise approach will be informed by building heights as set out in section 4.6. Opportunities for bespoke refurbishment of the existing coach house blocks to 1 or 2 bedroom properties at the western boundary of the site also exist.

### Affordable housing

4.5.4 The policy requirement for affordable housing is a minimum of 40%. The Council is considering a spectrum of delivery models which could include a number of different formats including co-operative housing.

### Community and other non-residential uses

4.5.5 Any planning application on the Depot site will need to be informed by an appropriate assessment of demand and capacity. The assessment should be in line with Policy 73:

Community, sports and leisure facilities in the emerging Local Plan. Consultation on the emerging proposals for Mill Road has pointed towards a need for new community uses and this will be considered in the context of a wider review of community resources by the Council which is due for completion in Summer 2016.

4.5.6 The exact approach will be dictated through the development of a more detailed brief. The City Council will work with partners to understand the following:

- The potential for reprovision of the Women's Resource Centre (currently occupying the building in the north-east corner of the site). It is important to consider whether co-location with other community activities is appropriate, or whether separate standalone facility is more suitable.
- The need for flexible community spaces such as a new hall or community meeting rooms will be considered. Opportunities to make connections with adjacent open space proposals should be sought.
- Demand and need for a nursery space will be explored as a potential "anchor" community use.
- Consider the aspirations of the Language School which is outside of the ownership of Cambridge City Council.
- Other acceptable non-residential uses could include a small proportion of bespoke workspace, for example, potentially arranged as live-work units as part of the refurbished coach house units.



**Fig 40** Examples of housing typologies in Cambridge of an appropriate scale and massing - examples include Accordia, Eden Street, Great Kneighton, Scholars Walk, Trumpington Meadows, Orchard Park, Pinehurst and Saxon Street





first floor



ground floor

**Fig41** Illustrative coach house redevelopment as mews



**Fig42** Existing coach houses

## 4.6 BUILDING HEIGHTS AND TYPES

### Building and housing types

- 4.6.1 A range of building and housing types should be provided across the site in a mixture of houses and flats. This range of typologies will help to create an integrated community, with homes suitable for a range of household types and sizes.
- 4.6.2 New homes should seek to optimise solar orientation and outlook, while also providing a robust street and block layout as illustrated in the site framework plan (figure 28). Dual aspect homes should be maximised and single aspect north facing homes avoided.
- 4.6.3 New homes should generally be set-back from the street, with small front gardens or privacy strips. Where possible, ground floor properties should have rear gardens, or patios and flats should have a balcony or terrace.
- 4.6.4 The level of parking provision will influence the mix of house types. For example, a high proportion of garages could reduce the proportion of houses, and require an uplift in apartments to meet the 167 unit target in the Local Plan allocation.
- 4.6.5 Opportunities exist to refurbish the existing coach house style block (see figure 42) at the western boundary of the site. This building could divide into a range of bespoke, small 1-2 bed homes with a "mews" character (see figure 41).



**Fig 43** Building heights



### Building heights

- 4.6.6 A range of building heights should be provided across the site to create visual interest and character (see figure 43). It is vital that detailed proposals respond to the wider context established in the Mill Road conservation area. The key characteristics are set out in chapter 2.
- 4.6.7 In broad terms, typical building heights are likely to follow a distinct east-west pattern across the site. The westerly line of building frontages, potentially involving the retained coach house-style buildings, proposed for refurbishment form a tight two storey edge at the western boundary of the site adjacent to private gardens to the rear of properties on Kingston Street. Buildings in the centre of the site will typically involve 2-3 storeys depending on the exact house types proposed. At the eastern edge of the site, it is proposed that the apartment buildings increase up to 4 storeys. Facades of any apartment building will need to be broken down to avoid the appearance of a long, horizontal edge to the railway line. A taller buildings, e.g. maximum of 5 storeys, could be appropriate at the south eastern edge of the site.
- 4.6.8 In general, proposals will be expected to incorporate architectural modulation and variety to generate a varied roofscape and positive streetscape / townscape character. This could be achieved through variations in massing along the street and within blocks, and innovative use of roof space for accommodation where appropriate.

### Heritage

- 4.6.9 Any planning application will be required to submit a townscape assessment and heritage statement in support of proposals. These assessments should demonstrate a thorough understanding of local context and key views. An initial appraisal of the 4 storey apartment buildings has been undertaken. Although few buildings in the conservation area reach four storeys it is considered that this scale is likely to be appropriate for the following reasons:
- Aside from the Grade II listed building and Gatehouse building, the Depot site does not make a positive contribution to the character and setting of the conservation area. It has major potential for change, and it is vital to set out a positive context for investment.
  - The proposed apartment blocks are situated at the edge of the conservation area and benefit from significant separation from existing streets and views at Kingston Street to the west (by virtue of the geographical separation and the likely screening effect of existing / proposed 2/3 storey properties). To the east, views from Great Eastern Street and Mill Road are mitigated by the horizontal distance associated with the railway, the raised road level of the bridge and the natural screening effect of the existing mature trees and planting off Mill Road.
  - The Council will seek to ensure that proposals are of the highest design quality and properly broken down to avoid long, horizontal facades.

### Boundaries

- 4.6.10 All new buildings should respect the amenity of surrounding existing properties and maintain a reasonable back-to-back distance between directly facing windows of habitable rooms in existing adjacent properties. Existing vegetation within the boundary should be retained where possible.



**Fig44** Illustrative plan



**Fig45** Long-term illustrative approach, showing the principle of the phased future redevelopment of the garages at the northern boundary of the site. This is outside of the 167 home allocation and would be beyond the Local Plan period

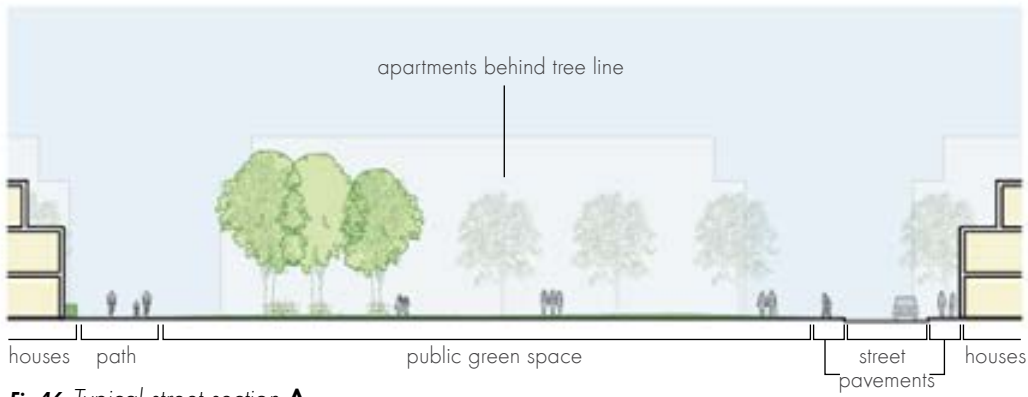
## 4.7 CHARACTER AND FORM

### Layout

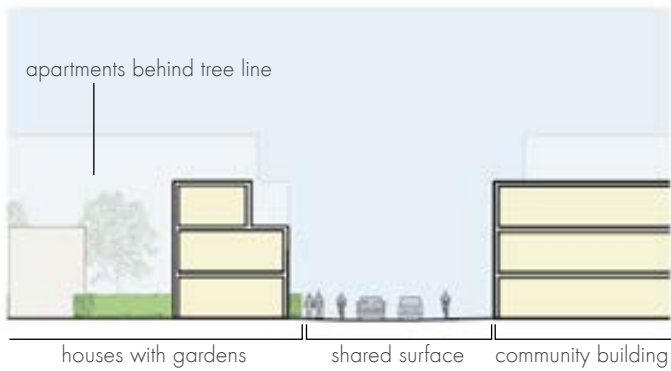
- 4.7.1 Section 4.2 provides an overview of the key principles in terms of site layout which is summarised in figure 6. The adjacent drawing (figure 44) provides an indicative illustration of the proposed approach. This illustrative masterplan does not have planning status for the purposes of the assessment of future applications, but provides a helpful illustration of how a scheme could come forward in response to the various parameters and supporting guidance in this document.
- 4.7.2 Existing streets and blocks should respond to the surrounding context as described in the Conservation Area Appraisal. A dominant north-south street pattern is suggested alongside more secondary east-west links. The street grain and approach to massing will play a key role in establishing a framework which responds to the terraced character and form that exists in streets in the surrounding conservation area.
- 4.7.3 Buildings should be oriented to overlook streets and public spaces, with layouts following the principles identified in best practice guidance such as Secure by Design.

### Long-term phasing

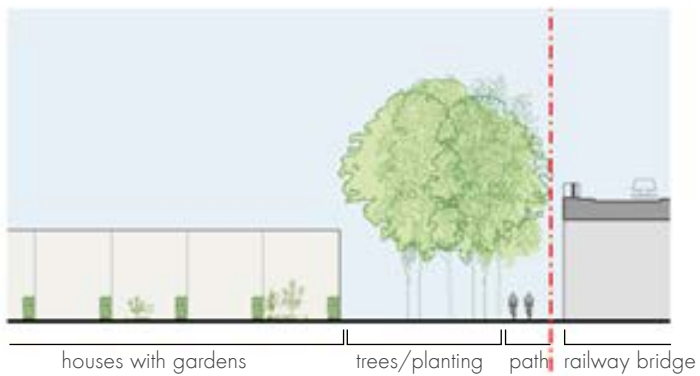
- 4.7.4 As illustrated on figure 45, the illustrative approach does not preclude the phased redevelopment of the existing garage blocks at the north-west corner of the site. Early work indicates that the 167 home target can be accommodated without redeveloping the garages. Subject to the expiration of existing long leaseholds, it would be possible to develop this parcel as a small collection of houses to address Hooper Street in a later phase beyond



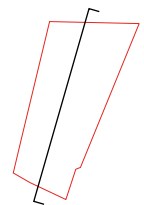
**Fig 46** Typical street section **A**



**Fig 47** Typical street section **B**



**Fig 48** Typical street section **C**



**Fig 49** Indicative North-South section through site

**Indicative street typologies**

- 4.7.1 Buildings should be oriented to overlook streets and public spaces, with layouts following the principles identified in Secure by Design.
- 4.7.2 Variety should be encouraged through careful design of street types with a clear distinction between public and private space and a hierarchy of street widths.
- 4.7.3 The following street types (figures 46 to 49 and 50 to 54 overleaf) illustrate indicative street typologies that could be developed at a planning application stage.





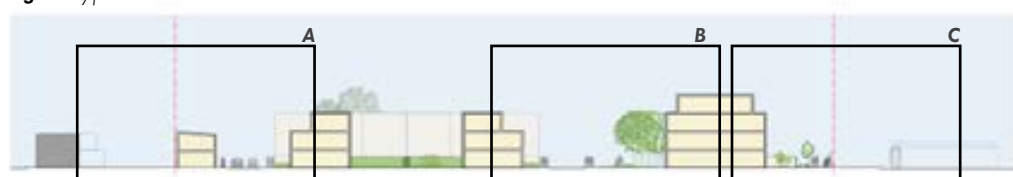
**Fig 50** Typical street section **A**



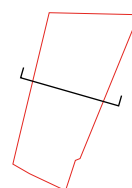
**Fig 51** Typical street section **B**



**Fig 52** Typical street section **C**



**Fig 53** Indicative East-West section through site

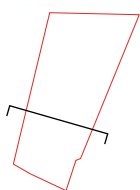


## Character

- 4.7.4 Proposals should respond to local townscape and demonstrate a positive and appropriate response to key design features including street elevations, roofscape, materials, massing and building corners. Opportunities for contemporary design solutions which consider to traditional local styles and detailing should be incorporated as appropriate.
- 4.7.5 Buildings with a larger form such as the apartments could draw inspiration from the scale and materiality of the historic industrial buildings in the wider Mill Road Conservation Area or indeed, some of the former buildings on the Depot site.



**Fig 54** Indicative West-East section through Southern end of site



## 4.8 ENVIRONMENTAL CONSIDERATIONS AND SUSTAINABILITY

### Site-wide sustainability

4.8.1 Creating a sustainable development should be a priority underpinning the redevelopment of the Mill Road Depot site. A site wide approach should be employed to address environmental, social and economic issues. Development should comply with the Cambridge Sustainable Design and Construction SPD and propose strategies for the following relevant issues:

- Health and well-being of future residents
- Energy efficiency of new buildings
- Design for climate change
- Water use
- Flood mitigation – SUDs
- Use of materials and resources
- Waste and recycling
- Employment opportunities
- Pollution
- Transport and mobility

4.8.2 Specific strategies that could be considered for the site include:

- Mitigation of overheating in flats and taking measures to improve internal thermal comfort.
- Water sensitive urban design such as sustainable drainage features like swales, rain gardens, ponds and rills could be designed into the open space and landscape.

4.7.3 The Local Plan allocation made reference to the provision of a district energy centre. More recent work has indicated that this might not be technically suitable for the site. A combination of passive design solutions and building design solutions should form the basis of site energy strategies for future schemes.

### Surface water drainage

4.8.4 Future proposals should consider the surface water drainage strategy at an early point in the design process. Initial work suggests that opportunities for storage / attenuation space should be provided as part of open spaces. These would be located close to existing discharge locations at the northern and southern parts of the site.

4.8.5 Above ground multi-functional storage could play an important role, together with a combination of sustainable drainage source control features such as rain gardens, permeable paving, swales, rills and green/brown roofs. Increased source control employed would mean that less underground storage will be required. On plot storage could provide around 5-10 cubic meters of storage per property if a parking space is provided.

4.8.6 Infiltration is the preferred method of surface water disposal and should be explored through detailed site investigations during the design development of the site; however soil types and the potential presence of contamination will possibly limit its use.

### Foul drainage

4.8.7 Foul drainage has similar discharge locations, both at the south and north of the site.

4.8.8 A split between north and south would be appropriate and although it is likely that capacity would be available in both of these, a pre-development enquiry should be made to Anglian Water to determine capacities and allowable discharge rates.

### Remediation

4.8.9 Elevated concentrations of various contaminants have been identified associated with made ground across the site, which would not be suitable for use within proposed domestic garden or landscaped areas. More localised contamination was identified within the

northern part of the site, especially surrounding the Underground fuel Storage Tanks (USTs).

- 4.8.10 Impacts to soil and groundwater from fuels were identified within this area which will require further assessment.

### Ecology

- 4.8.11 There are likely to be opportunities to enhance the ecology and biodiversity of the site. Initiatives should be developed in the context of wider plans for improvements to ecology and biodiversity.

- 4.8.12 Initiatives that could be considered are:

- Tree and other planting where appropriate (see section 4.3 above).
- Water resources in association with SUDs and landscape features where possible
- Nesting opportunities for a variety of bird and bat species
- Habitats for insects

- 4.8.13 Flat and low pitched roofs could provide an opportunity to improve the ecology of the site and contribute to the general increase in biodiversity. Both green and brown roofs with local species of flora might be desirable including roof allotments. The installation of photovoltaics over green roofs is also possible as the shade will add variety to the environmental conditions.

- 4.8.14 Opportunities for the creation of green walls comprising climbing plants could be explored in north and east facing walls to provide nectar sources for invertebrates and cover and night roosting sites for birds. Initiatives that link the provision of ecological enhancements with the public art strategy could also be explored.

- 4.8.15 A Phase 1 habitat and protected species

survey should be undertaken to establish which habitats and species are likely to occur. Particular focus should be on bat roost potential of any of the existing trees and buildings and invasive non-native species. This report would recommend if any further survey effort is required. BS42020 should be followed.

- 4.8.16 The sites should be viewed in the wider context to see how ecological connectivity between surrounding gardens, the railway line and new on site habitats may be enhanced during the master planning process. This should include detail such as gaps in garden fences to provide access for hedgehogs.

- 4.8.17 The SUDs design guide principles should be promoted to encourage the use of green, brown, biodiverse roofs, swales and attenuation ponds with ecological and recreational value.

- 4.8.18 Open Spaces and gardens should include native and non-native species trees, shrubs and herbaceous plants to provide a wide range of nesting, roosting and foraging opportunities throughout the year. There is potential to engage new residents with information packs on wildlife gardening principles.

- 4.8.19 As many units as practically possible, depending on scale, should have at least one integral bird or bat box for a range of species. Romsey retains a good Swift population that should be relatively easy to encourage to adopt new nest sites within the development.

- 4.8.20 The new residents will be well placed to use the new Chisholm trail and existing Sustran route to the south to explore Milton Country Park, RSPB Fen Drayton, Clay Farm Green Corridor and Trumpington Meadows Country park. On site interpretation or homeowner welcome pack could promote these natural sites.

## **4.9 PLANNING OBLIGATIONS**

- 4.9.1 The development is likely to result in increased demands for community infrastructure such as public open space, sports, health and community facilities and additional school and nursery places. Some of these demands will be met on site and others via commuted sums to provide new or enhanced infrastructure off site. It is also possible that in addition to the provision of the Chisholm Trail through the site, other transport improvements will be needed off site. Waste and Recycling facilities may also need to be secured. Planning Obligations via a Section 106 agreement will be needed to deliver this infrastructure. The full list and scope of these Planning Obligations will be defined through the consideration of the planning application(s).



## APPENDIX A: GLOSSARY OF TERMS

- **Biodiversity:** The number and variety of plants and animals.
- **Built form:** Buildings and their structures.
- **Cambridge Local Plan 2006:** This is the currently adopted Local Plan which sets out the policies and proposals for developments within Cambridge up until 2016. It includes a number of detailed policies and allocations where the Council would like new development to occur.
- **Cambridge Local Plan 2014 Proposed Submission:** Provides the policies and proposals for accommodating future developments within Cambridge up until 2031. The Plan is currently the subject of an independent examination. If found sound, the Plan will be adopted and will at that point replace the 2006 Local Plan. At this stage, this emerging document is in draft form only. It includes a number of detailed policies and draft allocations setting out how and where the Council would like future development to occur. One such draft allocation is the Mill Road Depot Site (Site R10).
- **Character and Form:** A combination of: the layout of buildings and streets; the height and appearance of the buildings; the amount and distribution of open space; and the density of a development.
- **Development principles:** A set of principles which underpin the redevelopment of the Mill Road Depot site.
- **Density:** Density is a method of measuring the intensity of development within a specified area. Density is calculated by dividing the number of homes by the site area in hectares. The site area includes roads and open spaces.
- **Framework Plan:** A plan used to illustrate how the open space, routes and building frontages work together on the Mill Road Depot site.
- **Habitats Regulation Assessment (HRA):** Habitats Regulations Assessments (HRA) are required under European Directive 92/43/ EEC on the “conservation of natural habitats and wild fauna and flora for plans” that may have an impact of European (Natura 2000) Sites. A HRA is the assessment of the impacts of implementing a plan or policy on a Natura 2000 Site. Its purpose is to consider the impacts of a Local Plan document against the conservation objectives of a site.
- **Hectare:** An area of 10,000 square metres
- **Legibility/Legible:** The degree to which a place can be easily understood and navigated.
- **Listed Building:** A building or structure of special architectural or historic interest and included in a list, approved by the Secretary of State. The owner must get Listed Building Consent to carry out alterations that would affect its character or its setting.
- **Local Plan:** Abbreviation used to describe the statutory plan adopted by the City Council.
- **Massing:** The combined effect of the arrangement, volume and shape of a building or group of elements. This is also called bulk.
- **Mitigation:** The purpose of mitigation is to avoid, reduce and where possible remedy or offset any significant negative (adverse) effects on the environment etc. arising from the proposed development.

- **Mill Road Conservation Area** There are 11 Conservation Areas within Cambridge, as defined by the City Council. These areas are considered to have 'special architectural or historic interest' and new developments within them must take this into consideration.
- **Parking Standards:** Document setting out maximum permissible levels of car parking for various land uses, along with minimum levels of cycle parking.
- **Planning Applications:** There are two possible approaches for the submission of a planning application. An 'outline' application establishes the broad principles of a development and sets development parameters, with more detailed matters submitted later as 'Reserved Matters' applications. Alternatively, a 'full application' would provide all details of the proposed development at the outset.
- **Public Realm:** The areas of city or town (whether publicly or privately owned) that are available, without charge for everyone to use or see, including streets, parks and open spaces.
- **Planning and Development Brief:** A planning policy document to help guide the preparation and assessment of future planning applications for specific sites coming forward for redevelopment.
- **Sustainability Appraisal (SA):** Sustainability Appraisal (SA) is a compulsory requirement under the 2004 Planning and Compulsory Purchase Act and the 2001/42/ EEC European Directive. A process used to appraise planning policy documents in order to promote sustainable development. Social, environmental and economic aspects are all taken into consideration.
- **Sustainable Development:** Sustainable Development is a broad term that encompasses many different aspects and issues from global to local level. Sustainable development can be described as 'Development, which meets the needs of the present without compromising the ability for the future generations to meet their own needs' (after the 1987 Report of the World Commission on Environment and Development – the Brundtland Commission).
- **Sustainable Urban Drainage Strategy (SuDS):** Sustainable urban drainage systems control and slow down surface water run off by mimicking natural drainage process in built-up areas. These systems include: areas for surface water storage; areas for water to infiltrate the ground slowly; and systems for limiting water flow.
- **Supplementary Planning Document (SPD):** SPDs were established as part of the Planning and Compulsory Purchase Act 2004 in United Kingdom law. They may cover a range of issues, be broadly thematic or site-specific. In the case of the Mill Road Depot site, the SPD is site specific and provides guidance on matters of design, land use and the amount of development appropriate for the site.

## **APPENDIX B: RELEVANT CAMBRIDGE LOCAL PLAN POLICIES**

Policy 1 Presumption in favour of sustainable development

Policy 3 Spatial Strategy for the location of Residential Development

Policy 5 Strategic Transport Infrastructure

Policy 26 Site Specific Development Opportunities

Policy 27 Carbon reduction, community energy networks, sustainable design and construction, and water use

Policy 31 Integrated water management and the water cycle

Policy 32 Flood Risk

Policy 33 Contaminated Land

Policy 34 Light Pollution Control

Policy 35 Protection of human health from noise and vibration

Policy 36 Air quality, odour and dust

Policy 45 Affordable Housing and dwelling mix

Policy 50 Residential Space Standards

Policy 51 Lifetime Homes and Lifetime Neighbourhoods

Policy 55 Responding to Context

Policy 56 Creating Successful Places

Policy 57 Designing new buildings

Policy 59 Designing Landscape and the Public Realm

Policy 60 Tall Buildings and the Skyline in Cambridge

Policy 61 Conservation and enhancement of Cambridge's historic environment

Policy 68 Open Space and recreation provision through new development

Policy 70 Protection of priority species and habitats

Policy 71 Trees

Policy 75 Healthcare facilities

Policy 80 Supporting Sustainable Access to Development

Policy 81 Mitigating the transport impact of development

Policy 82 Parking Management

Policy 85 Infrastructure delivery, planning obligations and the community infrastructure levy

Appendix B - Proposals Schedule Site R10 - Mill Road Depot

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# **Mill Road Depot Draft Planning and Development Brief Supplementary Planning Document (SPD)**

## **Statement of Consultation**

### **Background**

The City Council as the Local Planning Authority has been working in partnership with Allies and Morrison<sup>1</sup> to prepare a planning and development brief for the allocation. The purpose of the Mill Road Depot Draft Planning and Development Brief Supplementary Planning Document (SPD) is to guide the redevelopment of the site allocation, which is allocated in the Cambridge Local Plan 2014: Proposed Submission document (as amended<sup>2</sup>). The SPD will form a material consideration that will be taken into account by the Council in determining any future planning applications for the site.

The draft SPD is structured in four chapters:

- Chapter 1 provides the background to the draft SPD including the planning context, the process of preparing the draft SPD and the status of the document;
- Chapter 2 illustrates and provides an analysis of the Mill Road Depot site and its surrounding area giving consideration to the site's history, land uses, transport, open space, building and character in order to identify opportunities and constraints for future redevelopment;
- Chapter 3 provides a vision for the site and a series of development principles to achieve this vision;
- Chapter 4 provides guidance on how the site should be developed, organised into the following themes: site layout, transport and access, open space, uses, building heights and types, character and form, and sustainability.

### **Preparation of the draft SPD**

The City Council as the Local Planning Authority has been working in partnership with Allies and Morrison<sup>3</sup> to prepare a planning and development brief for the allocation. As part of this process, there have been two stages of informal local consultation held, the findings of which have been recorded in an Event Record prepared by Allies and Morrison Architects and dated 17<sup>th</sup> February, 2016. The first of these workshops was held in November 2015, and focused on issues and priorities for the site. This event provided an opportunity for local residents and stakeholders to make clear their aspirations (and concerns) for any redevelopment of the site and helped inform subsequent work by agents for the council as well as council officers. The second stage of consultation took place in January 2016 and involved consultation on draft

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<sup>1</sup> Allies and Morrison have been appointed on behalf of the City Council as a land owner.

<sup>2</sup> Through the *Addendum to the Cambridge Local Plan 2014 Proposed Submission Document (July 2013): Schedule of Proposed Changes Following Proposed Submission Consultation (February 2014)*

<sup>3</sup> Allies and Morrison have been appointed on behalf of the City Council as a land owner.

## **Appendix B: Statement of Consultation**

planning and design principles for the site. Again a workshop format was used and a significant amount of feedback was collected once more, this time on more detailed ideas for various “themes” for development (landscape and open space, movement, built form, etc.). All City and County Council members for Romsey and Petersfield wards were invited to both consultation events. In addition, a number of stakeholder groups were invited such as the Mill Road Society, Petersfield Area Community Trust (PACT), Mill Road Co-ordinator, Romsey Garden Club, Romsey Garden Project, Cam Cycle, Cambridge Past, Present & Future, Glisson Road and Tenison Road Area Residents Association (GTARA), South Petersfield Residents Association (SoPRA), Cambridge Association of Architects, Cambridge Ethnic Community Forum (CECF) and the Argyle Street Housing Co-operative (ASH co-op).

The emerging Mill Road Depot SPD has sought to address these comments constructively and creatively and balance the practical need of site delivery with the context of the local housing market and the council’s own objectives.

### **Consultation on the draft SPD**

The draft SPD was approved for consultation at Development Plan Scrutiny Sub Committee on 14 March 2016.

It is proposed that a public consultation takes place on the draft SPD after the local elections in May. This will be for six weeks, running from mid-May to late June 2016, with precise dates to be confirmed in due course.

A Sustainability Assessment and Habitats Regulations Assessment Screening Report have been carried out and consulted upon for the emerging Cambridge Local Plan 2014. This consultation took place between 19 July and 30 September 2013. These documents, along with other supporting documents will also be made available to view during this consultation. As the draft SPD supports the Cambridge Local Plan, there is no further need to undertake a separate Sustainability Appraisal or Habitats Regulations Assessment for this document, although screening reports have been completed and will be made available during the consultation.

### **Consultees**

The following organisations (below) have been directly notified of the Mill Road Depot Draft Planning and Development Brief SPD in accordance with the Town and Country Planning (Local Planning) (England) Regulations 2012 (as amended) via email or post where no email address is available (individuals are not listed).

## **SPECIFIC CONSULTATION BODIES<sup>4</sup>**

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<sup>4</sup> Specific consultation bodies and duty to cooperate required under the Town and Country Planning (Local Planning) (England) Regulations 2012 as amended



## Appendix B: Statement of Consultation

- Historic England
- Environment Agency
- Highways England
- Marine Management Organisation
- Natural England
- Network Rail
- Cambridgeshire Constabulary
- Cambridgeshire Police and Crime Commissioner
- Cambridge Fire and Rescue Service
- Cambridgeshire County Council
- Comberton Parish Council
- Coton Parish Council
- Cottenham Parish Council
- Fen Ditton Parish Council
- Fulbourn Parish Council
- Girton Parish Council
- Grantchester Parish Council
- Great Shelford Parish Council
- Hauxton Parish Council
- Histon & Impington Parish Councils
- Horningsea Parish Council
- Madingley Parish Council
- Milton Parish Council
- Orchard Park Community Council
- Teversham Parish Council
- National Grid
- UK Power networks
- Cambridgeshire Association to Commission Health
- Cambridgeshire and Peterborough Clinical

- Commissioning Group
- Cam Health
- Cambridge University Hospital NHS Foundations Trust (Addenbrooke's)
- NHS Property Services
- CATCH
- East of England Strategic Health Authority
- British Gas
- EDF Energy
- E.On Energy
- Npower Renewables
- Scottish Power
- Scottish & Southern Electric
- BT Openreach
- BT Group Plc
- Cambridge Water Company
- Anglian Water
- Homes and Communities Agency
- Office of Rail Regulation
- Civil Aviation Authority
- Greater Cambridge Greater Peterborough Local Enterprise Partnership
- Sport England

### COUNCILLORS

- 42 x City Councillors
- All County Councillors (City Wards)

### COMMUNITY ORGANISATIONS

- Age Concern Cambridgeshire
- Cambridge Citizens Advice Bureau
- Cambridge Ethnic Community Forum
- Cambridge Federation

- of Residents' Associations
- East Mill Road Action Group
- Romsey Action
- Mill Road Society
- Cromwell Road Residents' Association
- Petersfield Area Community Trust
- Mill Road Community Improvements Group
- Cambridgeshire Older Peoples Enterprise (COPE)
- Cambridgeshire Voluntary Sector Infrastructure Consortium (CVSIC)
- Disability Cambridgeshire
- East of England Faiths Council
- The Church of England Ely Diocese
- Cambridge Past Present and Future

### DEVELOPERS/AGENTS

- Barton Wilmore
- Beacon Planning Ltd
- Bidwells
- Carter Jonas
- Cheffins
- Home Builders Federation
- RPS
- Savills

### OTHERS

- Cambridge Cycling Campaign
- Cambridgeshire Campaign for Better Transport

## **Appendix B: Statement of Consultation**

**Other methods of notification** include:

- a public notice in the Cambridge News;
- through the Council's webpages <https://www.cambridge.gov.uk/mill-road-depot-spd>;
- via facebook: <https://www.facebook.com/camcitco>;
- twitter: <https://twitter.com/camcitco> and;
- the Council's local plan blog: <http://cambridgelocalplan.wordpress.com/>.

### **Consultation Methodology**

A six-week consultation period for the Mill Road Depot Draft Planning and Development Brief SPD will take place from:

**9am on x May 2016 to 5pm on x June 2016**

The draft SPD will also be made available for inspection along with the other relevant documents at the following locations:

- online on the council's website <https://www.cambridge.gov.uk/mill-road-depot-spd>
- at the council's Customer Service Centre at Mandela House, 4 Regent Street, Cambridge, CB2 1BY from 8am-5.15 pm Monday and 9am-5.15pm Tuesday to Friday.

The draft Mill Road Depot SPD will also be available for purchase from the Customer Service Centre (phone 01223 457200).

Comments can be made using:

- the online consultation system <http://cambridge.idi-consult.net/localplan/> or;
- the printed response form which is available from Customer Service Centre (details above) or can be downloaded and filled in electronically by visiting website <https://www.cambridge.gov.uk/mill-road-depot-spd>

Completed forms can be returned to:

- Planning Policy, Cambridge City Council, PO Box 700, Cambridge, CB1 0JH
- Or emailed to [policysurveys@cambridge.gov.uk](mailto:policysurveys@cambridge.gov.uk)

Respondents can request to be notified of the adoption of the document.

Contact details for further information were also made available as follows:

- Tel: 01223 457200
- Email: [policysurveys@cambridge.gov.uk](mailto:policysurveys@cambridge.gov.uk)

## **Appendix B: Statement of Consultation**

### **Next steps**

After the close of consultation, the key issues raised will be considered by the council and changes made to the draft SPD where appropriate.

The emerging Local Plan is still at the examination stage. If changes to the Local Plan 2014: Proposed Submission policies are made as part of the examination, the SPD will need to be updated to reflect these changes. For more information on the Local Plan examination, visit: <https://www.cambridge.gov.uk/local-plan-review-examination>.

The emerging Local Plan is still at the examination stage, which means that the council is unable to adopt the Mill Road Depot Draft Planning and Development Brief as an SPD until the Local Plan has been found sound and adopted. With this in mind, the planned adoption of the Mill Road Depot Draft Planning and Development Brief SPD will take place at the same time as the Cambridge Local Plan 2014.

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To: Executive Councillor for Planning Policy and Transport  
Report by: Director of Environment  
Relevant scrutiny committee: Development Plan Scrutiny Sub 14/3/2016 Committee  
Wards affected: Market Ward

### **PARK STREET CAR PARK PROPOSED PLANNING GUIDANCE NOTE**

Not a Key Decision

#### **1. Executive summary**

1.1 Cambridge City Council own and manage Park Street Car Park. The proposal to demolish the existing structure and redevelop the site to provide a replacement car park, residential and commercial uses have been agreed by the Council's Strategy and Resources Committee. The following requirements for the redevelopment of the site are as follows:

- 250 space underground car park
- Above ground, a mixed use development of market, intermediate and social housing, including an option for commercial rental on the ground floor, in conjunction with cycle parking
- The same or greater number of cycle parking spaces

1.2 At the meeting of the Strategy and Resources Committee on 12<sup>th</sup> October, 2015, the Leader of Council requested a planning brief be prepared for the Park Street Car Park. Appendix A of this report sets the Planning Guidance Note for Park Street Car Park.

#### **2. Recommendations**

2.1 This report is being submitted to the Development Plan Scrutiny Sub-Committee for prior consideration and comment before decision by the Executive Councillor for Planning Policy and Transport.



- 2.2 The Executive Councillor is recommended:
- a) To agree the content of the Planning Guidance Note for Park Street Car Park attached at Appendix A;
  - b) To agree that if any amendments are necessary, these should be agreed by the Executive Councillor in consultation with Chair and Spokes of Development Plan Scrutiny Sub Committee.

### **3. Background**

- 3.1 As members may be aware, there have been public reports as well as a public consultation pertaining to the future of the Park Street Car Park in the past few years. The Council has decided to continue to explore the redevelopment potential of the existing car park with the potential for a scheme that includes both residential uses along with re-providing a public car park (albeit with fewer parking spaces likely) and the same number or more of public cycle parking spaces. The request for planning guidance comes therefore as a result of the council's aspirations to better understand, and so respond to, the constraints and opportunities of the site and its surroundings (provide links to reports).
- 3.2 The purpose of the planning guidance note (attached at Appendix A) is to summarise clearly and concisely the planning context and constraints for the site as well as establishing key development principles for the redevelopment of the Part Street Car Park site.
- 3.3 The planning guidance note is organised around a number of key headings including:
- a) An analysis of the site history and its location
  - b) Establishment of key development and design principles
  - c) Existing and proposed scale and massing
  - d) Identification of key views and sensitive boundaries
- 3.4 The guidance follows the assumptions the Council is currently using to assess the different ways of delivering the redevelopment of the site, including a 250-space underground car park; a development of mixed tenure residential use, including the potential for ground floor commercial space; and a re-provision of the public cycle parking on site.

- 3.5 The existing car park is not considered a high quality building within the context of Park Street or Round Church Street. In fact the council's own Core Area Conservation Appraisal notes that the car park represents a development opportunity and is currently not an attractive part of this part of the historic core. As such, an opportunity exists for a new development to better respond to the immediate context of this part of the historic core of the city.
- 3.6 The key "headlines" of the guidance note may be summarised as follows:
- a) Scale and massing. The guidance suggests that an urban design led approach should be adopted to inform future redevelopment proposals but that it is likely that a majority 4-storey development will be appropriate on the site. Buildings should however step down to 2-storeys adjacent the Maypole Public House to allow a view through to St. John's Tower to be created and the Park Street frontage should be 3-storeys to better respond to the prevailing scale and massing of Park Street. Finally, the guidance notes there may be opportunity for an increase in height adjacent the Cambridge School of Visual and Performing Arts.
  - b) Rediscovering the "Yards". There were two previous yards which are a key part of the past history of the site, defining a particular character and informing the surrounding street pattern: Jordan's Yard and Ram Yard. It would be possible to continue Jordan's Yard further north-east from its current alignment in order to help reinforce a key route from the site; equally a notion of the former Ram Yard fronting on to Round Church Street) could be established along this frontage.
  - c) Public Realm. The redevelopment of this site affords the chance to improve the public in and around the site, including the potential to integrate existing trees.  
Private Amenity Space. The guidance notes that all new residential units should have access to private amenity space in the form of roof gardens, balconies and/or winter gardens of a usable size.  
Public Cycle Parking. The guidance notes that public cycle parking will be replaced with a new facility of the same or greater size (currently 274 spaces).
- 3.7 Due to the timescales of this project, it was considered appropriate for a guidance note to be prepared. Future consultation will be undertaken as the project moves forward and as part of the planning application process.

- 3.8 As noted in the January 2016 S&R minutes, the Council will be considering a future report which will consider a variety of issues with regards to the development of car park, including project costs, capital and revenue projections, proposals for affordable housing, parking and transport mitigation strategy during construction, and a project timetable and communication plan. This planning guidance note, if agreed by the Executive Councillor, will provide a clear starting point for more detailed discussions on planning work relating to the redevelopment of the Park Street Car Park site.

## **4. Implications**

### **Financial Implications**

- 4.1 There are no significant financial issues arising from the preparation of this guidance note. Staffing resources are already committed through the budget and service plan process.

### **Staffing Implications**

- 4.2 There are no direct staffing implications arising from this report.

### **Equality and Poverty Implications**

- 4.3 The guidance, once agreed, will help support a new development which should have a positive impact by ensuring the site in future is better integrated into the immediate and wider area. New development within the site should provide access for people with limited mobility.

### **Environmental Implications**

- 4.4 The redevelopment of the Park Street Car Park site should provide for the development of energy efficient buildings as well as the inclusion of renewable and low carbon energy generation.

### **Consultation**

- 4.5 The guidance has been prepared internally by officers. More detailed consultation will take place as the project moves forward and as part of the planning application process.

### **Community Safety**

- 4.6 There are no direct community safety implications arising from this report.

## **5. Background papers**

5.1 These background papers were used in the preparation of this report:

- Cambridge Local Plan 2014: Proposed Submission (as amended)  
<https://www.cambridge.gov.uk/local-plan-review-proposed-submission-consultation>
- Cambridge Core Area Conservation Appraisal

## **6. Appendices**

- Appendix A: Park Street Car Park Planning Guidance Note

## **7. Inspection of papers**

To inspect the background papers or if you have a query on the report please contact:

Author's Name: Glen Richardson

Author's Phone Number: 01223 457374

Author's Email: Glen.Richardson@ cambridge.gov.uk

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# Park Street Car Park

## Planning Guidance Note



**CAMBRIDGE**  
CITY COUNCIL



■ Front cover picture by Matthew Paul

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# 1. INTRODUCTION AND BACKGROUND

## 1.1. Introduction and Background

**1.1.1.** Cambridge City Council own and manage Park Street Car Park. The proposal to demolish the existing structure and redevelop the site to provide a replacement car park, cycle park, residential and commercial uses have been agreed by the Council's Strategy and Resources Committee. External consultants have advised on the viability of the redevelopment of the site which has created the following requirements as summarised in the report 'Future of Park Street Car Park' taken to Strategy and Resources Committee on the 18th January 2016. This follow on from the Strategy and Resources Committee (12th October 2015) where the Leader of the Council approved the following requirements for the redevelopment of Park Street Car Park:

- 250 space underground car park
- Above ground, a mixed development of market, intermediate and social housing, including an option for commercial rental on the ground floor, in conjunction with cycle parking
- The same or greater number of cycle parking spaces

## 1.2. Purpose and Scope of the Brief

**1.2.1.** The purpose of this planning guidance note is to summarise clearly and concisely the planning context and constraints as well as to establish key development principles for the redevelopment of the Park Street Car Park site. This guidance will help to ensure that the best and most efficient use of the land is delivered and will help with the future disposal of the

site through providing clear information on the form of development likely to be acceptable. It will assist Officers tasked with making recommendations on future redevelopment proposals and Members tasked with making decisions on planning applications.

**1.2.2.** Whilst this guidance provides a thorough understanding of the key planning matters relating to this site, it cannot account for subsequent changes in policy at a national or local level or in guidance and standards that may occur beyond the point of publication and approval. It should also be noted that this brief does not repeat existing guidance or policies but instead will make reference to other documents and advice where relevant. Future development proposals will need to be consistent with approved national and local policies in place at the time of the determination of planning applications. It will be for the site promoter to retain suitably qualified expertise to advise them of all planning related matters.



Figure 1: View of Park Street Car Park looking north-west

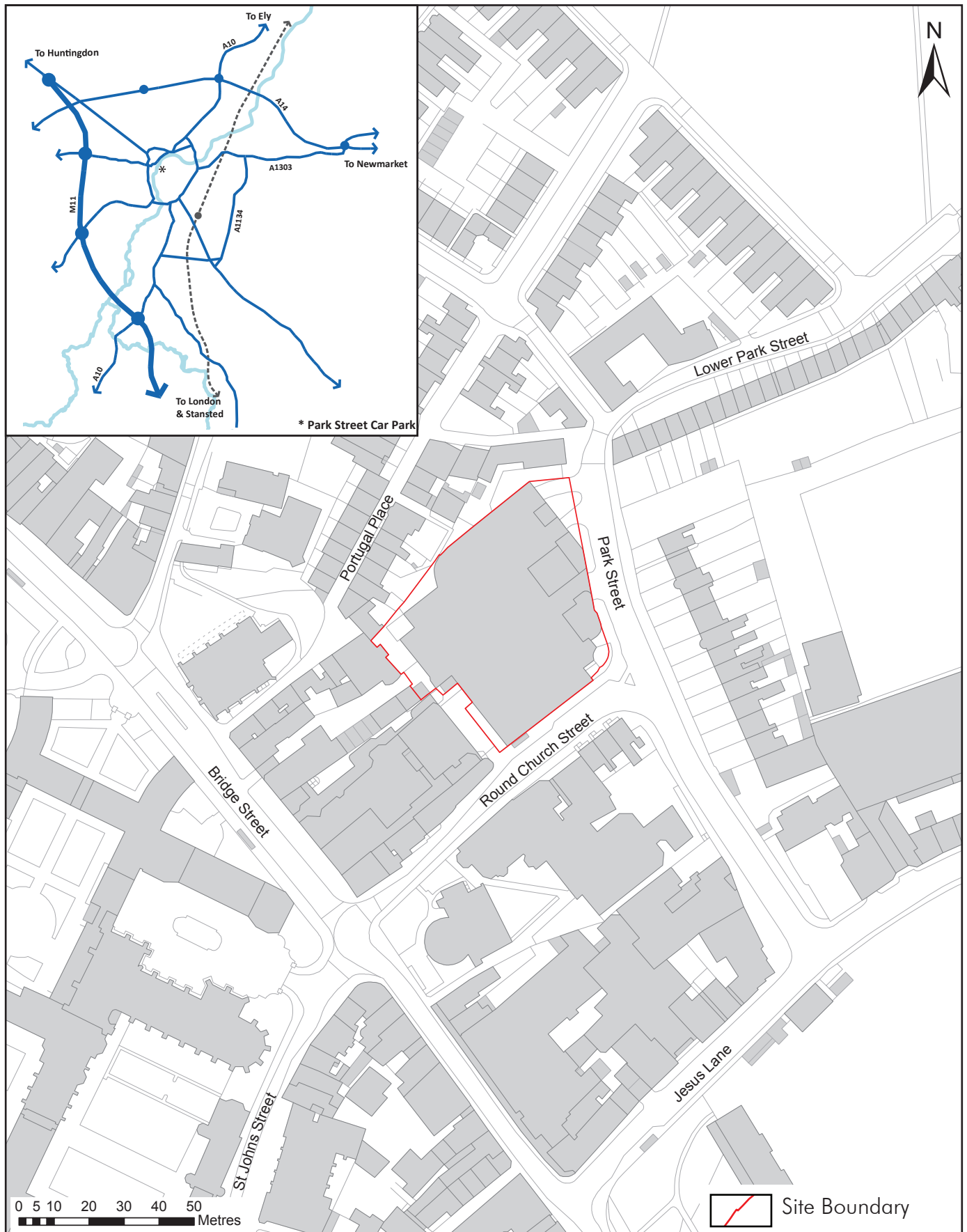


Figure 2: Site Location plan

## 1.3. Site Location, History and Analysis

### Site location

- 1.3.1.** Park Street car park is located at the junction of Round Church Street and Park Street within the Central Conservation Area and is identified in Figure 2: Site Location plan.

### Site Area

- 1.3.2.** The site is 0.3Ha in size.

### Policy designation

- 1.3.3.** The site is not allocated in the Cambridge Local Plan 2006 or the submitted Local Plan (2014) and can therefore be classed as a windfall site.
- 1.3.4.** The National Planning Policy Framework (NPPF) contains a presumption in favour of sustainable development and accordingly the redevelopment should make best use of the land available.

### Land ownership

- 1.3.5.** Park Street car park is currently owned by Cambridge City Council. It intended that The Council retain ownership of the site for the future redevelopment through a development agreement, joint venture or acting as a developer.

### Existing land use

- 1.3.6.** The site is currently occupied by a 390 space multi-storey car park, 274 cycle parking spaces, public toilets and a bicycle repair shop.

### Cambridge Union Society

- 1.3.7.** At the time of writing proposals for the redevelopment of the Cambridge Union Society site are under discussion.

### Site history

- 1.3.8.** The site is located within the Historic Core of Cambridge and as such has been intensively developed and redeveloped over the centuries. By the time the 1886

Ordnance Survey was published, the site can be seen to have a number of small units creating a well-defined edge to Round Church Street and Park Street. The now lost Ram Yard can be clearly seen and it is notable how many buildings are accessed from it.

- 1.3.9.** The 1926-7 Ordnance Survey map shows the site developed as a series of small units grouped around Ram Yard and Jordan's Yard with frontage onto Park Street and Round Church Street. The latter street being much reduced in width when compared with today.
- 1.3.10.** A road widening scheme in 1962 resulted in the narrow row of buildings being removed and the consequential loss of Ram Yard. The 1964 Ordnance Survey shows that the site had been comprehensively redeveloped to create the multi-storey car park devised as part of the Holford Plan (1950) for Cambridge and the re-engineering of the Medieval City to meet the burgeoning needs of the private motor car.
- 1.3.11.** Today the site is still in use as a multi-storey car park. A refurbishment of the car park was carried out in 1997 with cycle parking added in 2002 and refurbishment of the toilets in 2003.



Figure 3: View from St. John's Street towards Round Church Street and Park Street Park Street Car Park



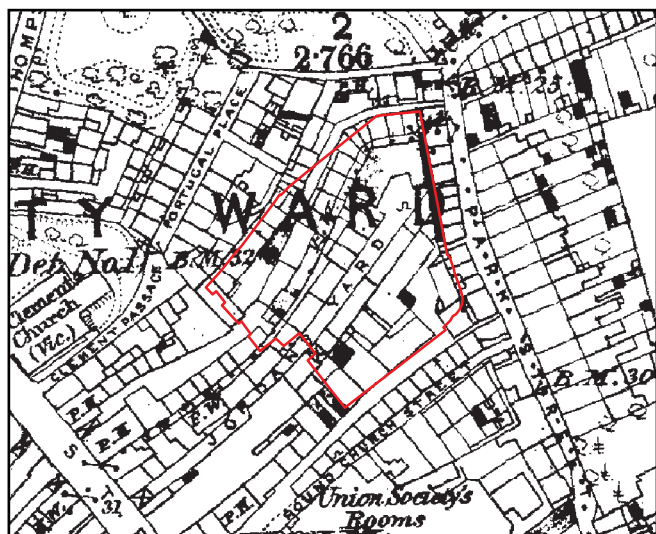


Figure 4: 1886 OS Map



Figure 5: 1926-27 OS Map

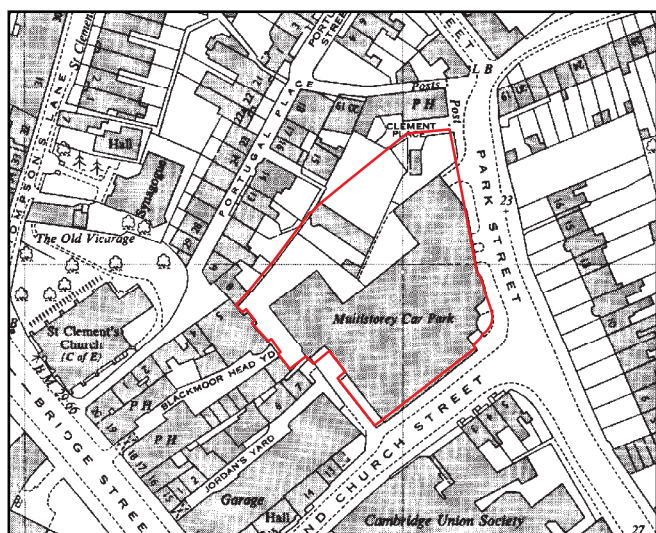


Figure 6: 1964 OS Map

**13.12.** Jordan's Yard still exists running in a north-east south-west direction and provides access to Park Street Car Park itself along with an entrance into Cambridge Arts & Sciences Sixth Form & Tutorial Collage (CATs). This route was somewhat truncated by the insertion of the car park and wider redevelopment of the area in the 1970s.

### Character Appraisal

**13.13.** To the north west of the site is Portugal Place, a street of high significance in terms of the character and quality of the Central Conservation Area. The 1950s Maypole public house bounds the northern site perimeter. To the south west boundary is CATs. A crane jib is attached to the northern elevation of this building and perhaps hints at the more industrial use of the wider site in the not too distant past.

**13.14.** The immediate locality shows the historically fine grain of the area with 'yard' type developments typical of the area. A number of these still exist

### Cambridge By Lewis No. 12

**Round Church Street.** This quaint old street with its wood and brick buildings, gay window boxes and cheerful paintwork, runs from Bridge Street to Park Street and it is from the latter entrance that our sketch is taken.



It is set in an area of "change," for long-standing demolitions are now changing the face of the once cobbled Park Street, and it is here, too, that the New Spine Relief Road is scheduled to run its concrete course.

Prziborsky's (established 1879) the old barber's shop, juts out like a lurching tram car at the other end of the street while in the middle of the pedestrian thoroughfare a solitary gas lamp stands sentinel over the central guttering, the best marble alley in all of Cambridge!

St. John's College Tower peeps over the shoulder of the houses, a happy knack it has throughout this part of the city.

Figure 7: Extract from Cambridge Evening News 1st September 1956 (Image courtesy of Cambridgeshire Collection, Cambridge Central Library)





Figure 8: View looking into Ram Yard, Cambridge Evening News 1960s (Image courtesy of Cambridgeshire Collection, Cambridge Central Library)



Figure 9: View of Ram Yard (left) and Round Church Street (right) from 1935 showing Prziborsky's hairdressing shop in the centre (Image courtesy of Cambridgeshire Collection, Cambridge Central Library)

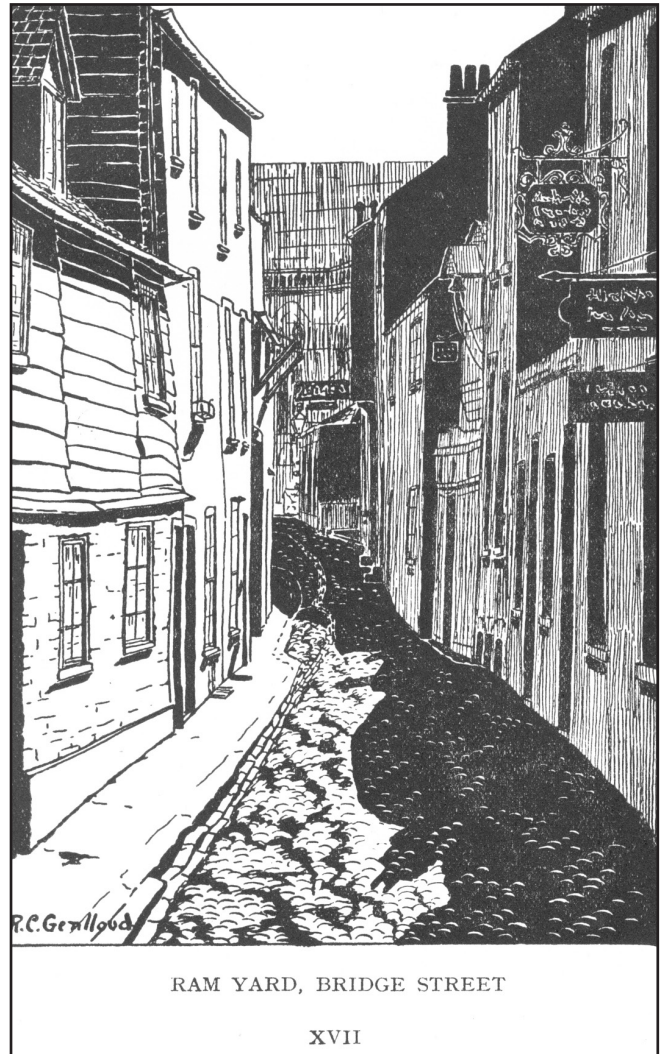


Figure 10: View looking north from Ram Yard by R.C. Genllood print, 1933 (Image courtesy of Cambridgeshire Collection, Cambridge Central Library)



Figure 11: View looking north from junction of Park Street and Round Church Street towards Lower Park Street 1920s (Image courtesy of Cambridgeshire Collection, Cambridge Central Library)





Figure 12: View from Ram Yard looking west towards St John's College, Cambridge Evening News 1960s (Image courtesy of Cambridgeshire Collection, Cambridge Central Library)



Figure 13: View of the Maypole Pub from corner of Portugal Place and Park Street, 1930s (Image courtesy of Cambridgeshire Collection, Cambridge Central Library)

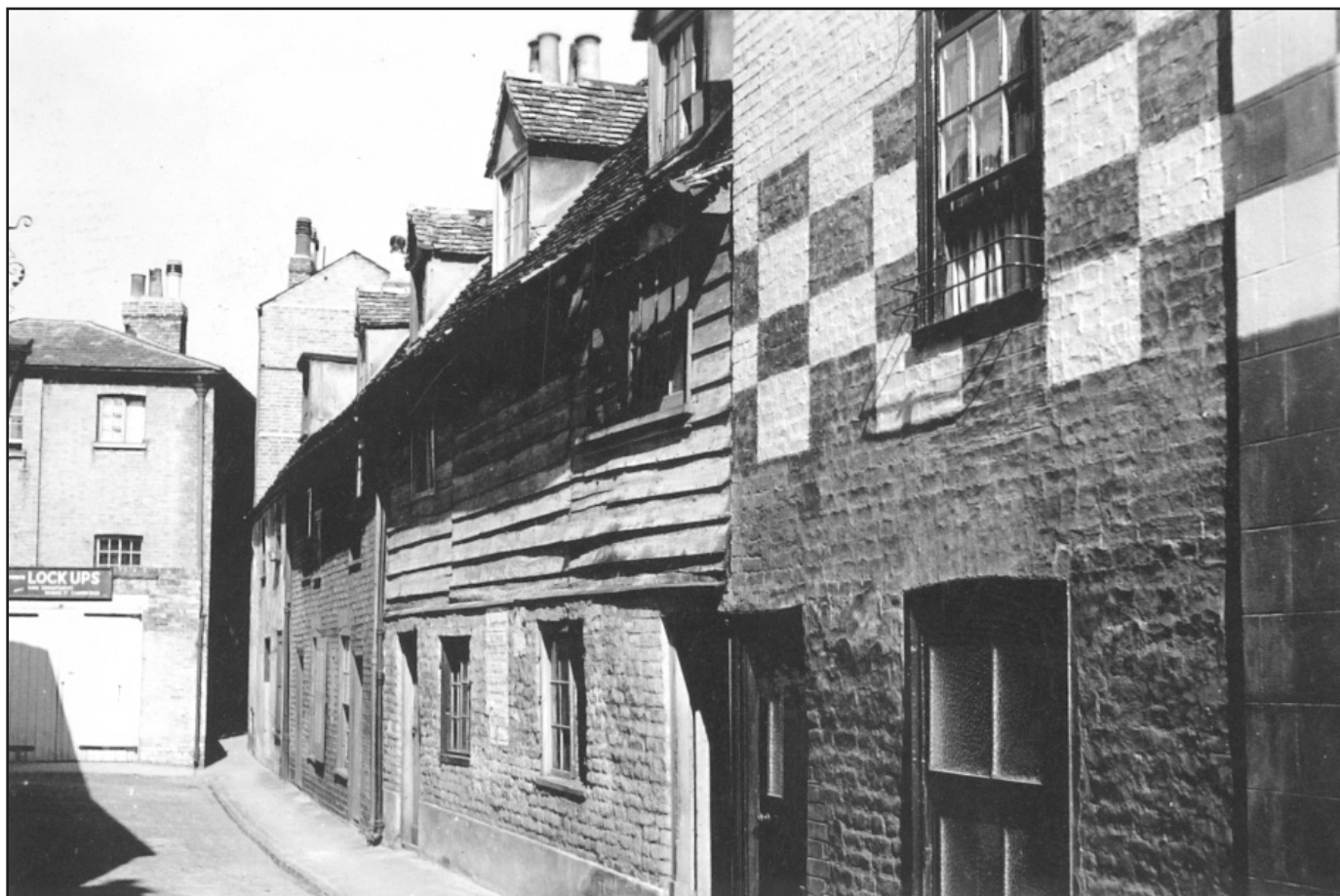


Figure 14: View of Duck-board house, Ram Yard 1937 (Image courtesy of Cambridgeshire Collection, Cambridge Central Library)





Figure 15: Former warehouse hoist on north-east elevation of CATs



Figure 16: View from Jordan's Yard looking towards Bridge Street



Figure 17: View from Blackmoor Head Yard looking towards Bridge Street

including the truncated Jordan's Yard that provides pedestrian access for the car park. Much of the fine grain development still exists and effectively sleeves the bulkier form of the former County Court building to the south-west of the site.

**13.15.** The car park itself is clearly a much larger format building and along with the office building (CATs) behind, sit somewhat incongruously in the area. Some attempt was made with car park to break down the overall scale and massing through the introduction of a rhythm of bays and variation in materials.

**13.16.** The Historic Core Appraisal recognises that the site creates a redevelopment opportunity. As a result, there is the potential to remove the 'back of house character' created by the existing car park.

### Designated and Non-designated Heritage assets

**13.17.** The site sits within the Central Conservation Area that affords statutory protection of the built environment within such an area. Therefore any future redevelopment must demonstrate that it will not harm the character and quality of the Conservation Area.

**13.18.** There are no listed buildings on the site although buildings on Portugal Place located to the north-west are Grade 2 Listed and so is the Cambridge Union Society to the south. Accordingly redevelopment proposals will need to demonstrate that they do not harm the setting of these buildings. Buildings of Local Interest at Nos. 5-7, 15 and 19-20 Portugal Place abut the site boundary.

**13.19.** Figure 12: Opportunities and Constraints, shows the location and proximity of Listed Buildings and Buildings of Local Interest in relation to the Park Street Car Park site.

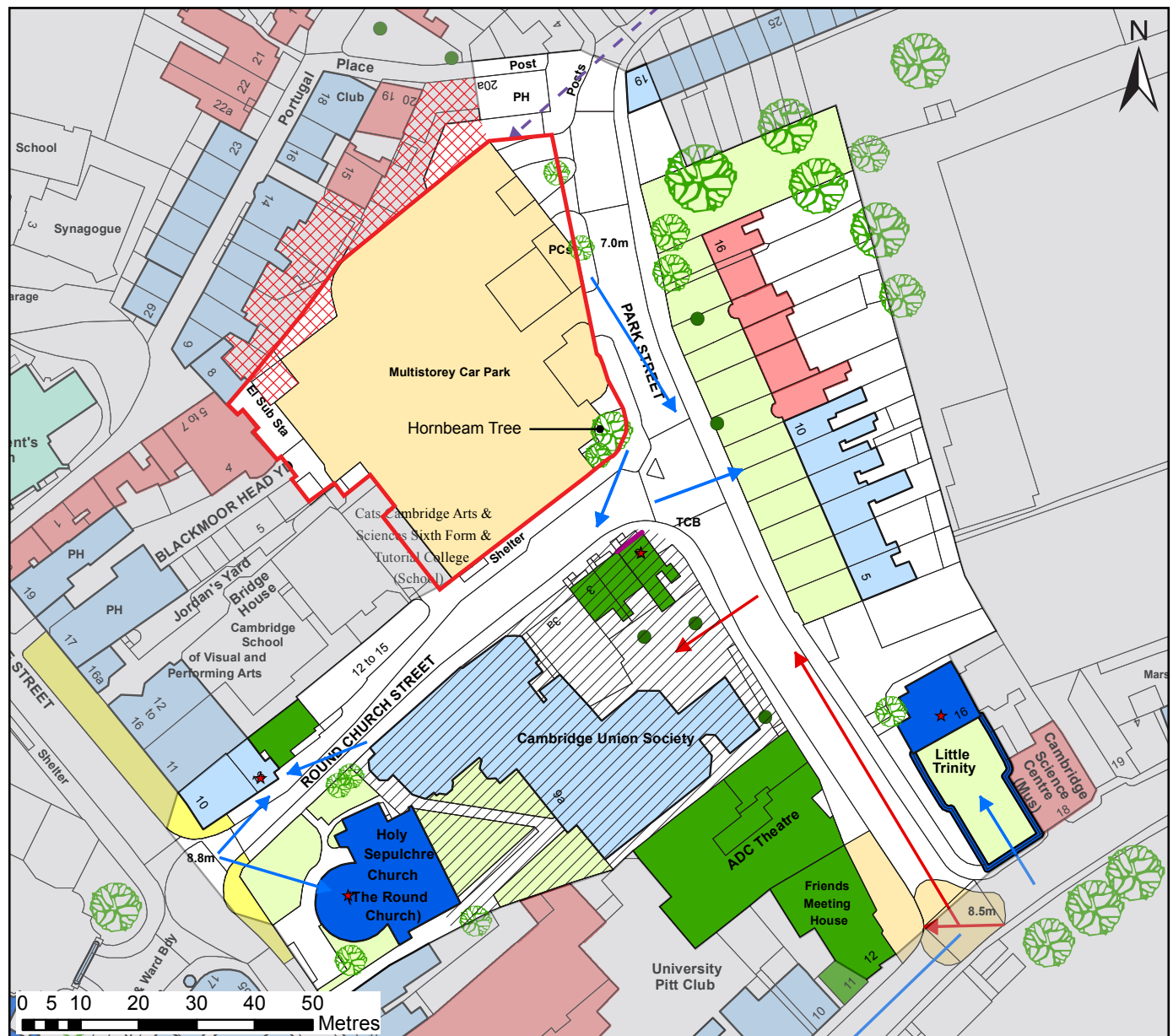
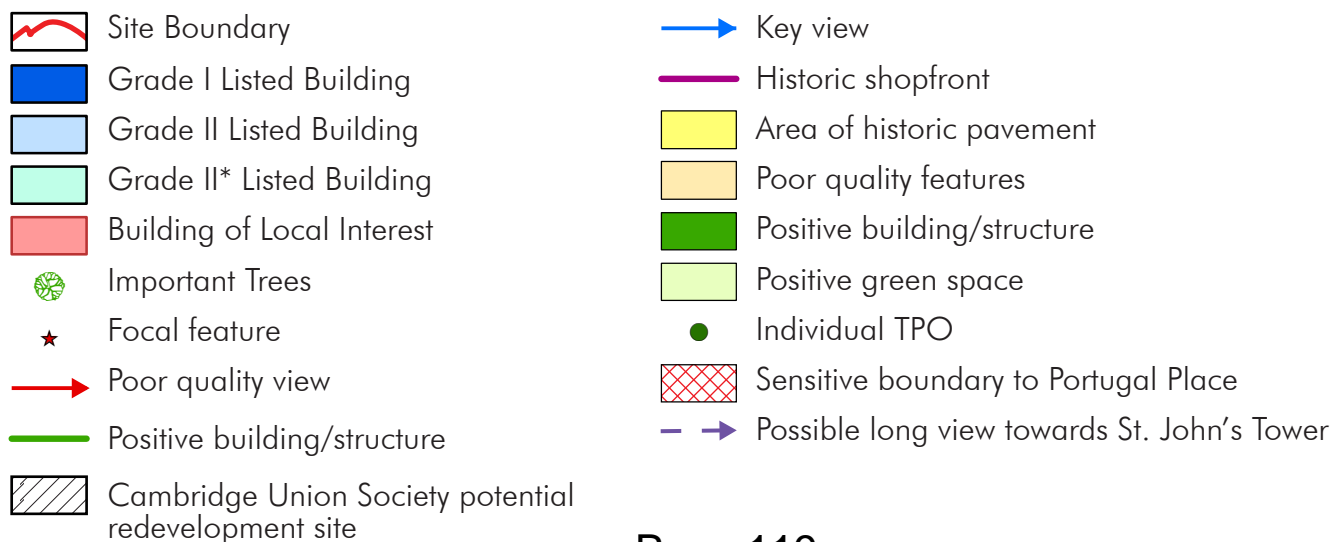


Figure 18: Opportunities and Constraints in the Park Street Car Park area



### Existing scale and massing

**1.3.20.** When compared to the historic grain of the area, the car park is a large format building that extends to 5 levels and an overall height of approximately 12m across the majority of the building. A taller stair core 'tower' occupies a position at the Round Church Street and Park Street junction and extends to approximately 15m. A second smaller stair core to a similar height occupies a position on the south-west facing section of the car park.

**1.3.21.** Figure 13 shows the existing scale and massing of the car park and prevailing scale and massing in the immediate area.

### Existing movement, circulation and access

**1.3.22.** Motor vehicle access and egress to the car park is from Park Street to the north. Access to cycle parking in the semi basement cycle park is from Park Street. In terms of pedestrian access, the site fronts onto both Round Church Street and Park Street, with access via stair and lift core on Park Street and from the south-west end of Jordan's Yard.

**1.3.23.** Future vehicle access locations will need to satisfy Cambridgeshire County Council as Highway Authority.

### Constraints and opportunities

**1.3.24.** Figure 12: Constraints and opportunities summarised the main constraints and opportunities to be considered for the redevelopment of the Park Street car park site.

### Tree Preservation Orders

**1.3.25.** There are no trees on the site covered by Tree Preservation Orders although the Hornbeam tree located to the junction of Park Street and Round Church Street should be assessed for condition and if feasible retained as part of future redevelopment.

### Archaeology

**1.3.26.** A limited archaeological investigation was carried out on the site in 2012. It is expected that a more detailed investigation will be required to establish the presence of archaeological remains. The likelihood of Roman and Medieval finds in this part of Cambridge is high and most usually at a depth of between 2 and 3 metres and given the proximity to the King's Ditch and the location of the site within the historic core, Cambridgeshire Archaeology at Cambridgeshire County Council should be contacted to establish the likelihood of archaeological remains being present on the site if necessary the best course of action for further work and investigation.

### Contaminated land

**1.3.27.** A Geotechnical and Contaminated Land Investigation was undertaken by RSK in 2013. It is expected that further investigations may be required as development proposals progress. Reference should be made to the pamphlet 'Contaminated Land in Cambridge – A Developer's Guide' which provides advice to all persons who are proposing to develop or who are involved in the development of land which may be affected by contamination.



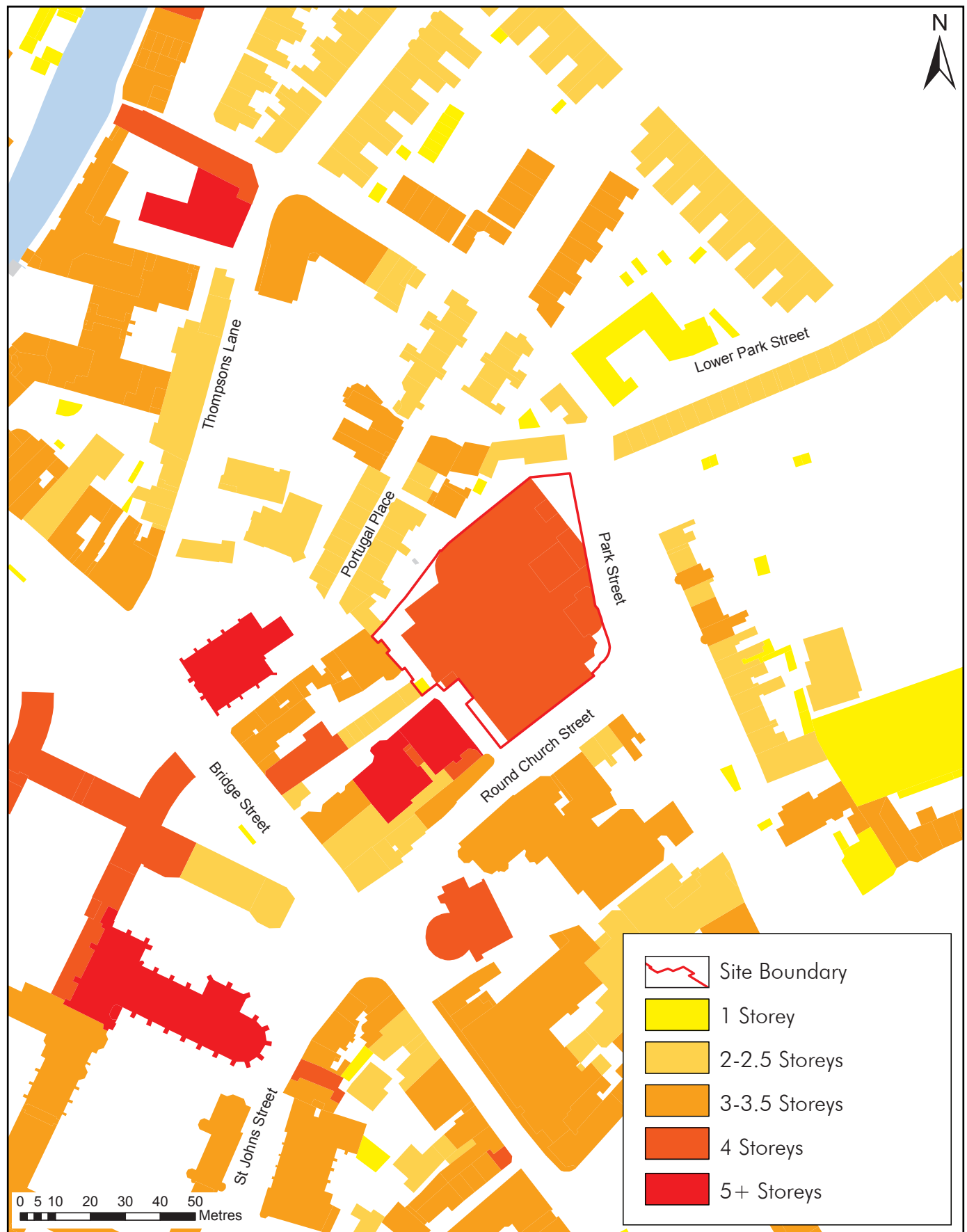


Figure 19: Existing Scale and Massing

## 2. PLANNING AND DESIGN ADVICE

### 2.1. Development principles

**2.1.1.** Key development principles are summarised in Figure 15. These will need to be considered when developing proposals for the Park Street car park site. In addition, further principles are established below.

### 2.2. Scale and massing

**2.2.1.** An urban design led approach should be adopted to inform the appropriate scale and massing of redevelopment proposals for the site. This should result in a well-designed scheme that fits into its context, helps to define key entrances and routes and responds to key views and vistas including views into and out of the site into the Conservation Area.

**2.2.2.** Building heights shown in Figure 15 are expressed as storey heights. It assumes that where commercial ground floor uses are proposed, the floor to ceiling height will typically be around 3.7m (floor to floor height 4m). Residential floors are assumed to have 2.7m floor to ceiling height (3m floor to floor height). Floor to floor heights assumes a 300-400mm construction depth for floors.

**2.2.3.** There will be some differences in floor to ceiling heights between buildings. These small changes are acceptable and indeed help to create greater variation in roofscape and overall massing.

**2.2.4.** It is likely that a majority 4 storey development will likely be appropriate on the site, subject to an acceptable design. Building heights should be stepped down to 2 storeys adjacent to the Maypole Public House to create a view to St John's College tower. The rest of the Park Street Frontage should be 3 storeys to better respond to the prevailing scale and massing of the area. There may be

the opportunity for a localised increase in height to the south-west (4 + 1 storeys) adjacent to the CATs building. The use of setback upper floors will be an effective way of moderating the overall scale and massing of the redeveloped site whilst creating well-articulated forms.

**2.2.5.** The use of setback upper floors on the two street frontages will allow for a scale of development more suited to the finer grain character of the surrounding area whilst optimising the land available.

**2.2.6.** Applicants will be expected to produce accurate 3D computer models to inform an appropriate massing of the development on any key views and vistas. Further advice is available in the City Council's Guidance for the Application of Policy 3/13 [Tall Buildings and the Skyline] of the Cambridge Local Plan 2006.



Figure 20: View towards the existing car park from Portugal Place

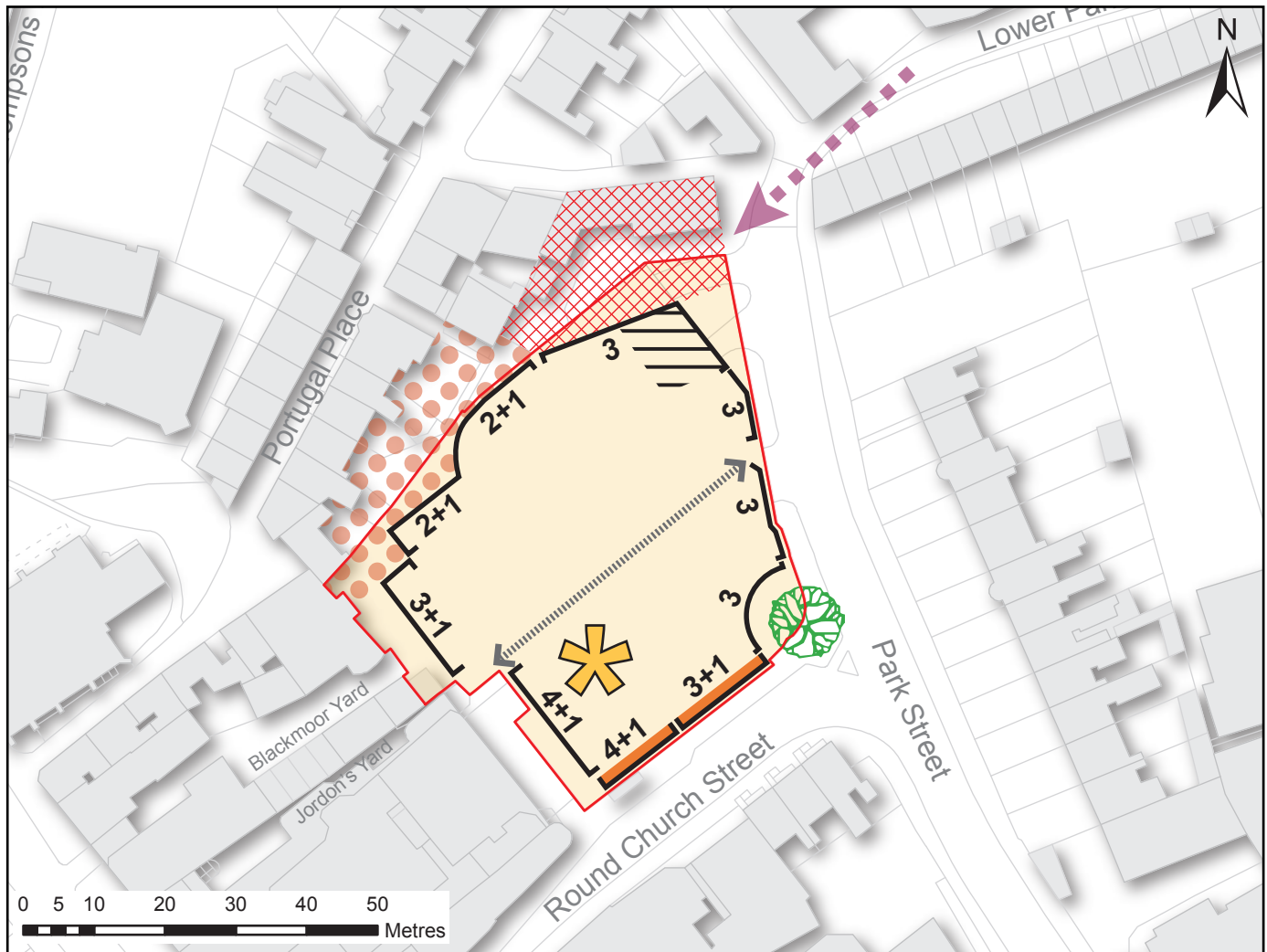












Figure 21: Development Principles

- |                                                                                     |                                                                                               |                                                                                     |                                                                                                                                                                                                                                                                                                                                                                                                 |
|-------------------------------------------------------------------------------------|-----------------------------------------------------------------------------------------------|-------------------------------------------------------------------------------------|-------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|
|  | Site Boundary                                                                                 |  | Height limited to 2 storeys to open up views towards St John's College Tower                                                                                                                                                                                                                                                                                                                    |
|  | Improve setting of pub and beer Garden                                                        |  | Indicative building frontages                                                                                                                                                                                                                                                                                                                                                                   |
|  | Design to resolve/mitigate potential neighboring residential amenity issues on Portugal Place |  | Location for possible retail frontage                                                                                                                                                                                                                                                                                                                                                           |
|  | Hornbeam tree retained if feasible and subject to condition assessment                        | <b><u>3+1</u></b>                                                                   | Storey heights - maximum storey heights indicated assume residential floor to ceiling height of 2.7m (3m floor to floor height). Assume ground floor commercial units would require a floor to ceiling height of 3.7m (4m floor to floor height). Overall height should be inclusive of plant. The +1 indicates accommodation in the roofspace or a setback upper floor (minimum setback 1.5m). |
|  | Possible for increased height adjacent to CATS (4+1 storeys)                                  |                                                                                     |                                                                                                                                                                                                                                                                                                                                                                                                 |
|  | Possible view to St. John's College Tower over rooftops                                       |                                                                                     |                                                                                                                                                                                                                                                                                                                                                                                                 |
|  | Possibility of reinstating Jordan's Yard to connect through to Park Street                    |                                                                                     |                                                                                                                                                                                                                                                                                                                                                                                                 |



## 2.3. Key views

**2.3.1.** The Historic Core Appraisal establishes the significance of the views looking north-east and south-east. In addition the view towards St John's College is a further significant view and the form of development and positioning of built form should respond to this view connecting the finer grain development along Lower Park Street visually through to the greater scale of this important building in the Historic Core of Cambridge.



Figure 22: view towards St. John's College Tower from Park Street



Figure 23: Potential view to St John's College Tower blocked by Park Street Car Park



Figure 24: Views towards St. John's College Tower from Portugal Place

## 2.4. Finer grain development

**2.4.1.** The basement car park allows for a more flexible form of development above ground that can respond more appropriately to the finer grain development of surrounding streets.

**2.4.2.** Proposals should investigate the possibility of reinstating the historic building lines on both Round Church Street and Park Street to work with the finer grain character of the area.

## 2.5. Rediscovering 'the yards'

**2.5.1.** Courtyard type schemes could be designed to reflect the historic character of the area. Jordan's Yard should be reconnected through to Park Street. The notion of Ram Yard could be reinstated.

## 2.6. Retail and commercial uses

**2.6.1.** If included as part of redevelopment proposals, retail uses could be located on Round Church Street which is the more active of the two frontages. It is envisaged that servicing for retail uses will be from the street.

## 2.7. Car park access

**2.7.1.** This will need to be well resolved to reduce the possibility of it negatively impacting on the quality and character of the street. Access is likely to be from Park Street but details will need to be agreed with Cambridge County Council as Highway Authority.



Figure 25: Views over surrounding streets showing finer grain of development

## 2.8. Public realm

**2.8.1.** The redevelopment of the Park Street car park should improve the quality of the public realm adjacent to the site boundaries. Where public realm is identified as of positive townscape value in the Core Appraisal, these areas should be sensitively integrated into new development proposals.

## 2.9. Amenity space

**2.9.1.** The development should ensure that all residential units have access to private amenity space in the form of roof gardens, balconies and/or winter gardens. It is essential that these amenity spaces are well designed and integral to the character of the development, are located so that they are comfortable to use and are of sufficient size. It is expected that private roof gardens, balconies and winter gardens should:

- Be large enough to accommodate a table and chairs

- Receive direct sunlight for part of the day
- Be positioned away from or designed to mitigate sources of noise and poor air quality

### Existing trees

**2.9.2.** The Hornbeam tree should be retained subject to feasibility and condition assessment.

## 2.10. Activate the edges

**2.10.1.** Making frontages 'active' adds life, vitality and interest to the public realm. To achieve this aim, development proposals will:

- Create well-designed entrances orientated on to the street to encourage activity within the public realm;
- Maximise the number and size of windows to increase natural surveillance; and
- Use features such as balconies, winter gardens, bay windows to enliven the frontages and articulate facades.

## 2.11. Cycle provision

**2.11.1.** Applicants should refer to the Cambridge City Council Cycle Parking Guide for New Residential Developments (February 2010). Cycle parking should be secure, well integrated and convenient to use and make provision for 'off gauge' or non-standard bicycles and trailers.

**2.11.2.** The public cycle parking will be replaced with a facility of the same or greater size.

## 2.12. Sustainable development guidelines

**2.12.1.** Creating a sustainable development should be a priority underpinning the redevelopment of the Park Street



car park site. A site wide approach should be employed to address environmental, social and economic issues. Development should comply with the Cambridge Sustainable Design and Construction SPD and propose strategies for the following relevant issues:

- Health and well-being of future residents
- Energy efficiency of new buildings
- Design for climate change adaptation
- Water use
- Flood mitigation including SUDs
- Use of materials and resources
- Waste and recycling
- Employment opportunities
- Pollution
- Transport and mobility

### Ecology

**2.12.2.** There will be many opportunities for enhancing the ecology and biodiversity of the site. Initiatives that could be considered are:

- Tree and other planting where appropriate
- Water resources in association with SUDs and other landscape features
- Nesting opportunities for a variety of bird and bat species
- Habitats for insects
- Brown or green roofs

### Drainage and flood risk

**2.12.3.** Part of the Park Street Car Park site is located within Flood Zone 2 on the

Environment Agency flood maps and the site is known to have a high groundwater levels. The lower section of the car park, currently used for cycle parking, has flooded in recent years. The historic alignment of the King's Ditch is in close proximity to the site.

**2.12.4.** When the site is redeveloped, it will be expected that attenuation will be required on site and given the proximity to the River Cam, such measures will be required to safeguard or improve the water quality leaving the site.

### Recycling and waste facilities

**2.12.5.** It is expected that any development which comes forward on the site will successfully integrate refuse and recycling facilities and clearly separate commercial and residential waste streams.

## 2.13. Public art

**2.13.1.** Public art is encouraged as part of development proposals in accordance with Policy 3/7 of the Cambridge Local Plan (2006). The engagement of an artist should be undertaken at an early stage of the design process to ensure that it is well integrated into proposals.

## 2.14. Planning obligations

**2.14.1.** The development is likely to result in increased demands for community infrastructure such as public open space, sports health and community facilities. Some of these demands may be met on site but others will be secured through commuted sums to provide new or enhanced infrastructure off site. Planning Obligations via a Section 106 agreement will be needed to deliver this infrastructure. The full list and scope of these Planning Obligations will be defined through the consideration of the planning application(s) for the site.

# Glossary

## Active frontages

An active frontage is one which allows some kind of movement or visual relationship between the person outside and the activity inside. At a minimal level, this interaction might be one of simple observation such as a window display or people working. At a higher level of interaction, the pedestrian could be encouraged to enter the unit to buy something or participate in an activity. The most interactive frontages are usually those of cafés, bars or shops, which spill out onto the street.

## Articulation

The expression of the vertical or horizontal subdivision of a building facade into perceivable elements by the treatment of its architectural features.

## Biodiversity

Encompasses all aspects of biological diversity, especially including species richness, ecosystem complexity and genetic variation.

## Building line

The line formed by the frontages of buildings along a street.

## Built form

Buildings and their structures

## Buildings of Local Interest

Buildings of Local Interest are not subject to statutory protection, but are recognised as being of importance to the locality or the City's historical and architectural development.

## Cambridge Local Plan 2006

The Cambridge Local Plan 2006 sets out policies and proposals for future development and land use to 2016; the Plan will be a material consideration when determining planning applications.

## Cambridge Local Plan 2014

The Cambridge Local Plan 2014 sets

out policies and proposals for the future development and land use to 2031; the plan will be a material consideration when determining planning applications.

## City Centre

Historic Core and Fitzroy/Burleigh Street shopping areas in Cambridge. These areas provide a range of facilities and services, which fulfil a function as a focus for both the community and for public transport. See also Cambridge Proposals Map (February 2008).

## Conservation Area

Areas identified, which have special architectural or historic interest, worthy of protection and enhancement.

## Fine grain

The quality of an area's layout of building blocks and plots having small and frequent subdivisions.

## Form

The layout (structure and urban grain), density, scale (height and massing) and appearance (materials and details).

## Historic Core Conservation Area Appraisal

The Historic Core Conservation Area Appraisal covers 70+ streets in the city centre which are defined according to their significance. This significance can be their historical, architectural or social impact on the character and appearance of Cambridge.

## Listed Building

A building or structure of special architectural or historic interest and included in a list, approved by the Secretary of State. The owner must get Listed Building Consent to carry out alterations that would affect its character or its setting.

## Massing

The combined effect of the arrangement, volume and shape of a building or group of elements.

### **Mixed use development**

Development comprising two or more uses as part of the same scheme. This could apply at a variety of scales from individual buildings, to a street, to a new neighbourhood or urban extension. 'Horizontal' mixed uses are side by side, usually in different buildings. Vertical mixed uses are on different floors of the same building.

### **Movement**

People and vehicles going to and passing through buildings, places and spaces.

### **Natural surveillance**

The discouragement to wrong-doing by the presence of passers by or the ability of people to see out of windows. Also known as passive surveillance.

### **National Planning Policy Framework (NPPF)**

The NPPF sets out the Government's planning policies for England and how these are expected to be applied. It sets out the Government's requirements for the planning system only to the extent that it is relevant, proportionate and necessary to do so. It provides a framework within which local people and their accountable councils can produce their own distinctive local and neighborhood plans, which reflect the needs and priorities of their communities.

### **Permeability**

Permeability describes the degree to which urban forms, buildings, places and spaces permit or restrict the movement of people or vehicles in different directions. Permeability is generally considered a positive attribute of urban design, as it permits ease of movement by different transport methods and avoids severing neighbourhoods. Areas which lack permeability, e.g. those severed by arterial roads or the layout of streets in cul-de-sac form, are considered to discourage effective movement on foot and encourage longer journeys by car.

### **Planning Obligation**

A binding legal agreement requiring a developer or landowner to provide or contribute towards

facilities, infrastructure or other measures, in order for planning permission to be granted. Planning Obligations are normally secured under Section 106 of the Town & Country Planning Act 1990.

### **Public Art**

Publicly sited works of art, which make an important contribution to the character and visual quality of the area and are accessible to the public.

### **Public Realm**

The parts of a village, town or city (whether publicly or privately owned) that are available, without charge for everyone to use or see, including streets, squares and parks.

### **Section 106**

See Planning Obligation.

### **Sustainable Development**

Sustainable Development is a very broad term that encompasses many different aspects and issues from the global to local levels. Overall sustainable development can be described as 'Development, which meets the needs of the present without compromising the ability for the future generations to meet their own needs' (after the 1987 Report of the World Commission on Environment and Development – the Brundtland Commission).

### **Sustainable Drainage Strategy (SuDS)**

Development normally reduces the amount of water that can infiltrate into the ground and increases surface water run-off due to the amount of hard surfacing used. Sustainable drainage systems control surface water run off by mimicking natural drainage process through the use of surface water storage areas, flow limiting devices and the use of infiltration areas or soakaways etc.

### **Urban Grain**

The pattern of the arrangement and size of buildings and their plots in a settlement; and the degree to which an area's pattern of street blocks and street junctions is respectively small and frequent, or large and infrequent.

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Photographs and images were taken or created by Cambridge City Council unless otherwise stated.

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Figure 5: 1926-27 OS Map

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Figure 15: Former warehouse hoist on north-east elevation of CATs

Figure 16: View from Jordan's Yard looking towards Bridge Street

Figure 17: View from Blackmoor Head Yard looking towards Bridge Street

Figure 18: Opportunities and constraints in the Park Street Car Park area

Figure 19: Existing Scale and Massing

Figure 20: View towards the existing car park from Portugal Place

Figure 21: Development Principles

Figure 22: View towards St. John's College Tower from Park Street

Figure 23: Potential view to St John's College Tower blocked by Park Street Car Park

Figure 24: Views towards St. John's College Tower from Portugal Place

Figure 25: Views over surrounding streets showing finer grain of development

Document prepared by: Cambridge City Council Urban  
Design & Conservation Team

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To: Executive Councillor for Planning Policy and Transport: Councillor Kevin Blencowe  
 Report by: Director of Environment  
 Relevant scrutiny committee: Development Plan Scrutiny Sub Committee 14 March 2016  
 Wards affected: All wards

## **NEW MUSEUMS SITE DEVELOPMENT FRAMEWORK - SUPPLEMENTARY PLANNING DOCUMENT**

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### **Not a Key Decision**

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#### **1. Executive summary**

- 1.1 The New Museum Site is a key University of Cambridge site, on the north side of Downing Street. It is allocated in the Cambridge Local Plan 2006 as Site 7.08 for redevelopment/refurbishment for predominantly University uses, with some mixed use to enhance the attractiveness of the public realm. Policy 43 University Faculty Development and Appendix B: Proposals Schedule in the submission Cambridge Local Plan (2014) identifies the New Museum Site as Site U2 for mixed-use redevelopment for university related uses.
- 1.2 The council and the University of Cambridge are working in partnership on the preparation of a New Museum Site Development Framework Supplementary Planning Document (SPD) to ensure that any future development on the New Museums Site safeguards the architectural, historic, cultural and archaeological importance of the site while providing an opportunity for positive qualitative change. The site development brief provides greater certainty and detail to support the delivery of this allocation in the coming years.
- 1.3 The emerging Local Plan is still at the examination stage, which means that the Council is unable to adopt the New Museums Site Development Framework as an SPD until the Local Plan has been found sound and adopted. In the interim period, prior to adoption of the SPD, the New Museums Site Development Framework provides context and guidance as material consideration in the planning process.

- 1.4 In accordance with the process of preparing an SPD, consultation on the draft Development Framework was carried out over a 6 week period between 13 July and 7 September 2016.
- 1.5 Appendix A provides a brief summary of the key issues raised during public consultation. Appendix B provides summaries of the representations received to the draft SPD and sets out the proposed responses. Appendix C provides a track-changed version of the SPD, whilst Appendix D incorporates the Statement of Consultation.
- 1.6 No significant changes are proposed as a result of the consultation. However some minor amendments to the SPD are proposed and are set out in Appendix C, reflecting the comments received during the public consultation set out in Appendix B.

## **2. Recommendations**

- 2.1 This report is being submitted to the Development Plan Scrutiny Sub-Committee for prior consideration and comment before decision by the Executive Councillor for Planning Policy and Transport.
- 2.2 The Executive Councillor is recommended:
  - To agree the responses to the representations received to the New Museums Site Development Framework (Appendix B) and the consequential amendments to the New Museums Site Development Framework(Appendix C);
  - To approve the New Museum Site Development Framework (Appendix C) in anticipation of the adoption of the Local Plan, and to agree that it should be carried forward for adoption as a Supplementary Planning Document at the same time as the Local Plan.

## **3. Background**

### New Museum Site SPD

- 3.1 The aim of the New Museums Site Development Framework SPD is to set out the joint aspirations of the council and the University of Cambridge regarding future changes to the site. These should improve the urban form with changes to the public realm, provide

better access for all and adopt more sustainable forms of development while respecting the site's heritage and surroundings. Future development on the site offers an opportunity to create an improved, more coherent development and especially to improve the public realm on the site.

3.2 The key issues and opportunities on the site have been identified as follows:

- Enhancement of the Historic Core Conservation Area and the careful management of existing built heritage assets on and around the site;
- Integration of the site into the wider city centre;
- Improvement of the external spaces within the site and creation of an appropriate public realm with improved permeability and public access especially to University museums and the heritage of the site;
- Improvement of University facilities in support of its continuing academic success and its importance to the life of the city;
- Increased environmental sustainability including better energy use, increased ecology and biodiversity and settings supporting the health and wellbeing of the site users and visitors;
- Reduced car parking and improved facilities for pedestrians and cyclists.

3.3 The New Museum Site Development Framework SPD has been written to support the new Cambridge Local Plan (2014). This SPD will be adopted at the same time as the Local Plan is adopted.

### Public Consultation

3.4 After being approved for public consultation at Development Plan Scrutiny Sub Committee (DPSSC) on 16 June 2015, the draft New Museums Site Development Framework SPD was the subject of consultation for 6 weeks between 13 July and 7 September 2016.

3.5 In line with the consultation standards set out in the council's Statement of Community Involvement and in accordance with the Town and Country Planning (Local Development) (England) Regulations 2004 (as amended), the consultation documents were sent to the statutory and other consultees. All of the consultation material was made available on the council's website and an online consultation system was utilised to allow people to submit their comments via the internet (hard copies of the response forms were made available to those who do not have access to the internet). A

notice was placed in the Cambridge Evening News on Monday 13 July 2015 containing information about the consultation and how people could get involved.

- 3.6 The draft SPD and its supporting documents remain available on the council's website at <https://www.cambridge.gov.uk/new-museums-site-development-framework-spd>

### Results of consultation

- 3.7 At the end of the consultation period, a total of 63 separate representations were received, made by 38 respondents, of which 4 (6%) were supportive, 23 (37%) were comments and the remainder, 36 (57%) were objections. The majority of responses came from the academic community however responses also came from a variety of other sources including local residents, businesses, planning consultants, the James Clerk Maxwell Foundation along with a number of other Clerk Maxwell enthusiasts, and Historic England.

- 3.8 In summary, the key issues raised (see Appendix A) include:

- The treatment of the site's permeability (approximately 30% of all the representations received);
- Concern over the treatment of and alterations to the Old Cavendish Laboratory and Mond buildings;
- Clarification and strengthening of particular paragraphs of the draft SPD;
- Historic England requested that minor changes be made to the text to clarify points of reference.
- Neighbouring organisations requested early consultation on proposals that may affect operational access and parking capacity/access.

- 3.9 Officers have worked through all representations and have drafted responses. Summaries of all representations and proposed responses with recommended changes to the SPD have been attached as Appendix B to this report.

### Next Steps

- 3.10 Subject to approval by the Executive Councillor, the public consultation responses will be published and loaded on the council's public consultation website.

- 3.11 In terms of status, the emerging Local Plan is still at the examination stage, which means that the council is unable to adopt the New Museums Site Development Framework as an SPD until the Local Plan has been found sound and adopted. In the interim period, prior to adoption of the SPD, the New Museums Site Development Framework provides context and guidance as material consideration in the planning process.

## **4. Implications**

### **Financial Implications**

- 4.1 There are no significant direct staffing or procurement issues arising from this report. Staffing resources are already committed through the budget and service plan process.

### **Staffing Implications**

- 4.2 There are no direct staffing implications arising from this report. The development of the SPD is already included in existing work plans.

### **Equal Opportunities Implications**

- 4.3 The SPD, once adopted, will have a positive impact by ensuring the site is better integrated into the wider city centre. New development within the site should improve access for people with limited mobility.

### **Environmental Implications**

- 4.4 The guidance contained within the SPD follows the principles of sustainable development as set out in the NPPF.

### **Consultation**

- 4.5 The consultation and communication arrangements for the SPD were carried as outlined in paragraphs 3.4 and 3.5.

### **Community Safety**

- 4.6 There are no direct community safety implications arising from this report.

## **5. Background papers**

- 5.1 These background papers were used in the preparation of this report:



- Cambridge Local Plan 2014: Proposed Submission (as amended)  
<https://www.cambridge.gov.uk/local-plan-review-proposed-submission-consultation>
- The New Museums Site Historic Environment Analysis (Beacon Planning Ltd., 2015)  
<http://democracy.cambridge.gov.uk/documents/s28744/CityCentreAccessStudy.pdf>
- Cambridge City Council Historic Core Area Appraisal (Cambridge City Council, 2006)  
<https://www.cambridge.gov.uk/historic-core-appraisal>

## 6. Appendices

- Appendix A: Brief summary of the key issues raised during public consultation
- Appendix B: Schedule of Consultation Representations
- Appendix C: New Museums Site Development Framework Supplementary Planning Document with tracked changes
- Appendix D: Statement of Consultation

## 7. Inspection of papers

To inspect the background papers or if you have a query about the report, please contact:

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## Appendix A: Brief summary of the key issues raised during public consultation

Chapter 1 – Introduction			
Respondents	Support (incl. qualified)	Object	Comment
5	2	3	0
(Support)	<ul style="list-style-type: none"> <li>Support 1st sentence in para 1.1.1 however the historical significance of the site is not being approached appropriately hence objection to para 1.4.1 and the lack of proper implementation</li> </ul>		
(Object)	<ul style="list-style-type: none"> <li>Failure to recognise and protect the old Cavendish Laboratory; contradictory to SPD's statement of intent.</li> <li>The old Cavendish Laboratory is a key piece of scientific history, the external view and style should be retained in line with 1.4.1b,c,d,e,f.</li> <li>The Mond and Cavendish laboratories should not be demolished;</li> <li>The Cavendish laboratory should become a museum.</li> </ul>		
Draft response and consequential changes to SPD	<ul style="list-style-type: none"> <li>It is not considered appropriate to single out individual buildings in the key issues section. There are many other significant buildings of heritage significance that would then need to be listed out herein, this is done later in the document at a more appropriate level of detail.</li> <li>Careful consideration and attention has and will continue to be given towards any alteration or restoration of the site's built heritage.</li> <li>Works to the Cavendish Laboratory as part of the Student Services Centre project, which also impacted the Arts School, were carefully assessed by the City Council and Historic England as part of application 15/0779/LBC and were approved in 2015.</li> </ul>		

Chapter 2 – Planning Context			
Respondents	Support (incl. qualified)	Object	Comment
2	0	1	1
(Object)	<ul style="list-style-type: none"> <li>Loses important connection with the Cavendish Lab history. It is important to preserve the Lab, designed by James Clerk Maxwell.</li> <li>The proposal ignores the historical significance of the Cavendish lab.</li> <li>Need to preserve history as an inspiration and challenge for future generations.</li> </ul>		
(Comment)	<ul style="list-style-type: none"> <li>When making reference to the fact that English Heritage has engaged with the preparation of the Masterplan, it would helpful to note in brackets that English Heritage is now known as Historic England (so as to avoid confusion with the English Heritage Trust).</li> </ul>		
Draft response and consequential changes to SPD	<ul style="list-style-type: none"> <li>Careful consideration and attention has been given towards the restoration of the site's built heritage. Works to the Cavendish Laboratory as part of the Student Services Centre project, which also impacted the Arts School, were carefully assessed by the City Council and Historic England as part of application 15/0779/LBC and were approved in 2015.</li> <li>Delete all references to "English Heritage" and replace with "Historic England".</li> </ul>		

Chapter 3 – Vision and Objectives			
Vision			
Respondents	Support (incl. qualified)	Object	Comment
1	0	0	1
(Comment)	<ul style="list-style-type: none"><li>• Specific detail needed for different retained buildings;</li><li>• Only the Goldsmith Laboratory should be demolished;</li><li>• Careful consideration needed for entrances through Arts School and Pembroke Street; and</li><li>• Holistic approach needed to house the entire Science Library Collection in a purpose built building and, or relocated to West Cambridge.</li></ul>		
Draft response and consequential changes to SPD	<ul style="list-style-type: none"><li>• Careful consideration and attention has been given towards the restoration of the site’s built heritage. Only the Mond annexe is scheduled for demolition, as agreed via application 15/0777/FUL &amp; 15/0779/LBC (the second application which consented to the new entrance through the Arts School);</li><li>• Plans to house the entire Science Library Collection altogether are under consideration.</li></ul>		
Objectives			
Respondents	Support (incl. qualified)	Object	Comment
7	1	4	2
(Support)	Objective 3: <ul style="list-style-type: none"><li>• Retention of Arts School supported although more detail needed including the new entrance from Bene’ts Yard and how it will relate to the renovated wing of the Arts School.</li></ul>		
(Object)	Objective 1: <ul style="list-style-type: none"><li>• The entire Science Library Collection should be housed in a purpose built building; and</li><li>• Buildings of significant historical and scientific importance should not be altered thereby retaining their historical connections</li></ul> Objective 4 <ul style="list-style-type: none"><li>• There is an internationally recognised context to Cambridge. That context is largely provided by the buildings. Without the buildings and connection to the past, there is no particular reason for the current and future university to be there at all. There are other rivers, other opportunities to create a 'new town' for the university if that is what is sought.</li></ul>		
(Comment)	Objective 3: <ul style="list-style-type: none"><li>• Add a final sentence to read along the lines “Some of the core buildings are of significance and those of greatest significance will be retained”. This will acknowledge that the Mond building is listed and will be kept but allow the Mond Annex (a Building of Local Interest) to be removed.</li></ul>		
Draft response and consequential changes to SPD	<ul style="list-style-type: none"><li>• Plans to house the entire Science Library Collection altogether are under consideration.</li><li>• Careful consideration and attention has been given towards the restoration of the site’s built heritage. Detailed consultation is undertaken with the Council, Historic England and others before</li></ul>		

	<p>any proposals are put forward which could impact on assets of heritage significance, either via their use or design.</p> <ul style="list-style-type: none"> <li>• Works to the Cavendish Laboratory as part of the Student Services Centre project, which also impacted the Arts School, were carefully assessed by the City Council and Historic England as part of application 15/0779/LBC and were approved in 2015. Only the Mond annexe is scheduled for demolition.</li> <li>• Agree to amend Objective 3 by adding an additional sentence at the end of this paragraph to read: "Some core buildings are of significance and those of greatest significance will be retained".</li> </ul>
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Chapter 4 – The Existing Site			
Respondents	Support (incl. qualified)	Object	Comment
10	0	6	4
(Object)	<ul style="list-style-type: none"> <li>• The development of the Cavendish Laboratory, or rather their partial demolition, would destroy the space in which much important science was carried out</li> <li>• The Cavendish Building is of international significance and its Grade II listing understates its intrinsic importance. That it was the first of its kind and that it was largely designed by James Clerk Maxwell put it into a category deserving of far greater respect than the scant regard paid to it by this scheme.</li> <li>• The Cavendish should be maintained in its entirety. This is a significant part of Cambridge's contribution to the history of Science. Piecemeal tinkering is not acceptable.</li> <li>• The need to demolish the Examination Hall should not be hurried. The Examination Building is connected with the Arts School building and the separation of them by means of demolition would be a serious operation which should be very carefully planned beforehand.</li> <li>• In this plan only part of the original Cavendish Laboratory is designated as being of Very High heritage significance, and no grounds are given for distinguishing the front of the building from the side wing, which held many significant teaching and research laboratories but is here designated as being only of High significance.</li> <li>• The proposal to create a new entry to the site through the side wing does not reflect the considerable value of retaining the integrity of the original laboratory building. The purpose built combination of teaching and research spaces was novel, and central to the success of the laboratory.</li> <li>• The Mond building should be reassigned to high importance.</li> </ul>		
(Comment)	<ul style="list-style-type: none"> <li>• For the sake of clarity (and to avoid confusion with the current Botanic Gardens) it is suggested that the final part of the sentence reads 'a good deal of the former Botanic Garden remained undeveloped.'</li> <li>• Suggest adding 'and Augustinian Friary' after King's Ditch at the end of paragraph 4.3.13.</li> <li>• Paragraph 4.6.1 states that access to the site is currently very poor. However, I question this and would suggest that what is very poor is the permeability of the site. If you compare plan 10, which</li> </ul>		

	<p>illustrates the current access points, with map 13, which shows the proposed access points, there is no change. Suggest that the opening sentence of paragraph 4.6.1 reads: 'Access into the site is currently poor and permeability through the site is very poor, as highlighted by Plan 10.'</p> <ul style="list-style-type: none"> <li>The Mond and Cavendish buildings are both of too great a historical significance to be partly destroyed.</li> </ul>
Draft response and consequential changes to SPD	<ul style="list-style-type: none"> <li>Careful consideration and attention has been given to the restoration of the site's built heritage. Works to the Cavendish Laboratory as part of the Student Services Centre project, which also impacted the Arts School, were carefully assessed by the City Council and Historic England as part of application 15/0779/LBC and were approved in 2015.</li> <li>Only the Mond annexe (a Building of Local Interest) is scheduled for demolition as agreed under application 15/0997/LBC. The removal of which will enhance the setting of both the Mond Building and the Cavendish Laboratory.</li> <li>The demolition of the Examination Hall has been approved via approval of application 15/0777/FUL.</li> <li>The significance of the Cavendish Laboratory has been carefully considered together with Historic England and the City Council and is considered appropriate.</li> <li>Agree to amend paragraph 4.2.6 to add the word "former" after the words "...of the..." in the final sentence.</li> <li>Agree to amend paragraph 4.3.13 to add the words "and Augustinian Friary" after the words "King's Ditch".</li> <li>For many users, especially cyclists and pedestrians, access is very poor and often the subject of conflict with vehicles, construction traffic, etc. Permeability is very poor too, however this is fully recognised and the parameters for change address this in a significant fashion. No change proposed to Paragraph 4.6.1.</li> </ul>

Chapter 5 – Parameters for Change			
5.1 – Connectivity and Movement			
Respondents	Support (incl. qualified)	Object	Comment
5	0	4	1
(Object)	<p>a. The Urban Block</p> <ul style="list-style-type: none"> <li>Agree congestion in the area is undesirable however the solution is not to open retail outlets in along Pembroke Street will diminish the non-commercial, academic character.</li> </ul> <p>b. Urban Connectivity</p> <ul style="list-style-type: none"> <li>Conditional support for enhanced connectivity if there are no future impacts or restrictions for the car parks / Grand Arcade and that access is not restricted as a result of the changes</li> <li>Oppose restriction on car parking that may impact upon operational requirements;</li> <li>Consultation requested to discuss public realm improvements and highway alterations in light of the site's proximity with the Grand Arcade and existing of car parks.</li> </ul>		
(Comment)	<ul style="list-style-type: none"> <li>Support pedestrians having priority, cyclists should also be given priority. Need to reduce vehicular traffic.</li> </ul>		



	<ul style="list-style-type: none"> <li>• Implications for signage and advertising need to be considered when testing the appropriateness of introducing small specialist retail outlets into Pembroke Street frontage.</li> </ul>
Draft response and consequential changes to SPD	<ul style="list-style-type: none"> <li>• The introduction of some specialist retail units at ground level along Pembroke Street is only an option as noted in paragraph 5.1.12. In any event permission for change of use would likely be necessary and would enable detailed assessment of whether such uses were appropriate.</li> <li>• Early consultation with neighbouring and, or potentially concerned site users, to be consulted prior to formal planning application submission to overcome cited concerns.</li> <li>• Works to the east side of the site on Corn Exchange Street are nearing completion, however early consultation with neighbouring and, or potentially concerned site users, to be consulted prior to any future formal planning application submission to overcome cited concerns.</li> <li>• Section 5.1 of the SPD sets out strategies to address issues of access and traffic management. Equally section 5.2 sets out improvements to the public realm to create significant new areas of open space will be achieved.</li> <li>• The implication for signage and advertising for small retail outlets would be given due consideration at the planning application stage.</li> </ul>

Chapter 5 – Parameters for Change			
5.1 – Connectivity and Movement c. Site permeability			
Respondents	Support (incl. qualified)	Object	Comment
18	1	16	1
(Support)	<ul style="list-style-type: none"> <li>• So long as "existing window openings [are] sensitively adapted and possibly enlarged" this is an appropriate compromise between past and present.</li> </ul>		
(Object)	<ul style="list-style-type: none"> <li>• The Cavendish Laboratory - one of the most scientifically significant buildings in the world - should be preserved as a monument to its achievements and systems of scientific learning;</li> <li>• Heritage value of the Cavendish Laboratory wing in understated;</li> <li>• The iconic laboratory should become a museum;</li> <li>• Proposed development will impact the front façade and the original design and layout of what was the first purpose built academic laboratories in the world. The new entrance will destroy the internal arrangement of the building, e.g. Rutherford's personal laboratory;</li> <li>• "Plain" exterior should celebrate the Laboratory's history while the other side is one of the most iconic scientific building in the world;</li> <li>• Proposals to destroy its core fabric are contradictory to the proposed Policies 60 and 61 in the emerging Cambridge Local Plan 2014 document;</li> <li>• More sympathetic development of the Cavendish that those submitted should be possible and investigated. See University of Coimbra for very good examples of sympathetic restoration and re-purposing of historic laboratories;</li> <li>• More clarity needed regarding the future treatment of the nearby buildings.</li> <li>• Alternatives should be considered rather than new entrances on</li> </ul>		

	Pembroke Street and Free School Lane.
(Comment)	<ul style="list-style-type: none"> <li>Do not support new entrances on Pembroke Street and Free School Lane. The current entrances are adequate for pedestrians and cyclists. Both streets are part of the few visually appealing streets left in the city centre. The proposal that new entrances would be sensitive to historical surroundings is vague and easily ignored in today's planning environment. The Bene't Street and Corn Exchange entrances could be widened and made the main public entrances. There are more pedestrians in that area anyway. The Whipple Museum main entrance could be in the new courtyard which is proposed.</li> </ul>
Draft response and consequential changes to SPD	<ul style="list-style-type: none"> <li>Careful consideration and attention has been given towards the restoration of the site's built heritage. Works to the Cavendish Laboratory as part of the Student Services Centre project, which also impacted the Arts School, were carefully assessed by the City Council and Historic England as part of application 15/0779/LBC and were approved in 2015. Only the Mond annexe is scheduled for demolition.</li> <li>Section 5.1 explains the objectives and constraints to the Pembroke Street access. It is made clear that the entrances are not adequate nor do they support the creation of a better connected, more legible environment for both public and university use. The other entrances are equally part of the wider site access arrangements that either have been, or will be, considered in detail. Careful consideration of the impact on building fabric and heritage assets will be undertaken before any works requiring planning permission are granted.</li> </ul>

5.2 – Open Space and Public Realm			
Respondents	Support (incl. qualified)	Object	Comment
9	0	3	6
(Object)	<ul style="list-style-type: none"> <li>It is not clear from this description what exactly will be done to the Mond Building, but it is an important historical site, both in its own right as a pioneering laboratory and as part of the Cavendish Laboratory complex.</li> <li>The Mond annexe was designed and built to be part of a suite which includes the Mond Laboratory. As such it cannot be considered as a separate and subordinate building. Photographs in the architectural press dating from the opening of the Mond Lab give just as much prominence to the annexe as to the tower and crocodile. There is no evidence that the architect H.C. Hughes or his contemporaries thought of the annexe as anything other than *part* of the suite of Mond buildings. It must be kept for the same reasons that the Mond Lab is being preserved.</li> <li>Oppose the destruction of these historic parts of the Old Cavendish laboratory and the Mond Laboratory for the sake of 'the public realm' for the following reasons: <ul style="list-style-type: none"> <li>Unsure that the lack of "clarity, cohesion, and spatial identity" is a bad thing, A tasteful, spacious solution to the cluttered feel of the New Museums Site can be found without demolishing the spaces where such incredible discoveries and scientific advances were</li> </ul> </li> </ul>		

	<p>made.</p> <ul style="list-style-type: none"> <li>- The conservation of these precious, culturally significant laboratories is more important to the public than the 'realm' their destruction might create.</li> </ul>
(Comment)	<ul style="list-style-type: none"> <li>• When it comes to the buildings in the centre of the square area, the central area would be enhanced by more pedestrianisation and trees in the centre which would justify the demolition of the centrally placed Rolls Royce Laboratory.</li> <li>• The adequate space provided within the central area (between the Austin Building and the Attenborough Building) will not require any demolition or removal of any part of the Austin Building.</li> <li>• The retained Mond Building fits with the increased pedestrianised space although the purpose of the reduced Mond Building is not indicated.</li> <li>• To the south of the area it is envisaged demolishing the Shell Building and the Goldsmiths' Laboratory to enable 'greening of the area around a new entranceway from Pembroke Street. Again this would contribute to the increased space between the Attenborough Building and the Old Metallurgy Building which it is intended to retain.</li> <li>• The necessity of demolishing both the Shell Building and Goldsmiths Laboratory to attain such a space is not clear. If the size of the Attenborough Building annexe is only as is almost completed at the moment then adequate pedestrian space by the removal of the Goldsmiths' Laboratory only and not the Shell Building as well would be a better and more realistic plan.</li> <li>• The making of a new entrance through the Heycock Wing into the central area should be very carefully considered as this frontage onto Pembroke Street is also of listed nature and the engineering difficult. Could not the existing entrance from Pembroke Street be slightly altered to serve as the entrance?</li> <li>• Recommend that signage is added to the list of items needing a consistent treatment in the design of opens spaces.</li> </ul>
Draft response and consequential changes to SPD	<ul style="list-style-type: none"> <li>• Demolition of the Shell Building or the Goldsmiths' Laboratory are required to achieve public realm as anticipated in the university master plan and which the SPD supports; and</li> <li>• Comments regarding careful treatment of the Pembroke Street façade are noted and any changes to this elevation will be considered as part of any works to the 1907 building. Note these buildings are not however statutorily listed.</li> <li>• Sufficient space has been catered for space across three new squares for a variety of uses.</li> <li>• Agree to add the words "signage" in paragraph 5.3.5 after the word "materials".</li> <li>• The Mond Building will be retained, only the Mond annexe is scheduled for demolition as approved under 15/0779/LBC.</li> <li>• Careful consideration and attention has been given towards the restoration of the site's built heritage. Works to the Cavendish Laboratory as part of the Student Services Centre project, which also impacted the Arts School, were carefully assessed by the City Council and Historic England as part of application 15/0779/LBC and were approved in 2015. Only the Mond annexe is scheduled</li> </ul>

	for demolition.
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<b>5.3 – Built Form</b>			
Respondents	Support (incl. qualified)	Object	Comment
3	0	0	3
(Comment)	<ul style="list-style-type: none"> <li>The exact nature of the new Student Services Building on the site of the Examination Hall is not completely specified although it seems to be envisaged as 3 storey and of ultramodern construction and style. Therefore the retention and care of the Arts School building should be seriously considered and planned for in detail from the beginning of work on the Examination Hall.</li> <li>When considering the impact of new buildings on the quality of external spaces through daylight and sun path modelling, both spaces within the New Museums Site and the adjacent public realm should be considered.</li> <li>Existing build creates operational difficulties for the Cambridge Corn Exchange.</li> </ul>		
Draft response and consequential changes to SPD	<ul style="list-style-type: none"> <li>The Arts School building is to be retained and incorporated into the new approved proposal for the Student Services Centre approved under 15/0777/FUL.</li> <li>Paragraph 5.3.5 does note the word “site” in reference to the public realm, hence no change to the text is considered necessary.</li> <li>Concerns duly noted however it is not for this SPD to seek to address existing operational difficulties with existing conditions. Co-ordination between the City Council and the University will be necessary as part of any new development and during construction and later operations phases.</li> </ul>		

<b>5.4 – Sustainability</b>			
Respondents	Support (incl. qualified)	Object	Comment
1	0	1	0
(Object)	<ul style="list-style-type: none"> <li>The conservation of historical assets is last on the list of sustainability considerations. Truly good plans would find a better balance of sustainability objectives.</li> <li>Struggle to see a substantial difference. Cambridge believes their rationale for destruction is simply even more righteous, but the end result in both cases is unnecessary and undesirable destruction.</li> </ul>		
Draft response and consequential changes to SPD	<ul style="list-style-type: none"> <li>Careful consideration and attention has been and will be given to the alteration of the site’s built heritage. Detailed consultation is undertaken with the council, Historic England and others before any proposals are put forward which could impact on assets of heritage significance, either via their use of design.</li> </ul>		

<b>5.5 – Phasing and Implementation</b>			
Respondents	Support (incl. qualified)	Object	Comment
1	0	0	1
(Comment)	<ul style="list-style-type: none"> <li>Careful joined-up planning of large vehicular access will be required in dialogue with the Cambridge Corn Exchange and Cambridge Arts Theatre to ensure that both arts venues can continue to ensure access for shows.</li> </ul>		

Draft response and consequential changes to SPD	<ul style="list-style-type: none"> <li>• Early consultation with neighbouring and/or potentially concerned site users would be necessary.</li> </ul>
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Chapter 6 – Planning Obligation			
Respondents	Support (incl. qualified)	Object	Comment
1	0	0	1
(Comment)	<ul style="list-style-type: none"> <li>• The Cambridge Corn Exchange (CCE) adjoins the New Museums Site and plays a vital function for both University and city. Suggest either a 'Planning Obligation' to ensure a contribution towards its capital development costs (in order to better serve both town and gown) or, preferably, an active partnership between the New Museums Site project team and the Cambridge Live team responsible for CCE to plan an ambitious programme of shared facilities and activities.</li> </ul>		
Draft response and consequential changes to SPD	<ul style="list-style-type: none"> <li>• Such a planning obligation would require detailed consultation and agreement, which would be outside the remit of this framework, or at the very least via a shared building or public realm project between the University and the council. The planning obligations set out in this SPD are considered appropriate and proportionate to the impact of the development on its surrounding, notably the need for future improvements in and around the main entrance to the site off Downing Street.</li> </ul>		



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**1. Introduction**

**Paragraph 1.1.1**

<i>Representation(s)</i>		<i>Nature</i>	<i>Appearance</i>	<i>Soundness Tests</i>
30709	Sam Smith [5762]	Support	Not Specified	None

*Summary:*  
I fully support the statement that "The New Museums Site (NMS), as shown in Plan 1 below, occupies a prominent location in Cambridge and has an extraordinary history as the historic centre of science in the University of Cambridge."  
  
However, that history is not being approached appropriately given the plans for the Cavendish, hence my objection to 1.4.1 . The goals of this submission are entirely laudible, it is the implementation that is currently lacking, but lacking in an entirely correctable manner.

*Response*  
  
Comments duly noted.  
Careful consideration and attention has and will continue to be been given towards any alteration or restoration of the site's built heritage.

*Action*  
  
No further action.

=====

<i>Representation(s)</i>		<i>Nature</i>	<i>Appearance</i>	<i>Soundness Tests</i>
30692	Dr. Roger Albin [5746]	Support	Not Specified	None

*Summary:*  
As a scientist with a strong interest in history of science, I urge you to preserve as much of Maxwell's pioneering design as possible. His implementation of laboratories for research and teaching were much more important than many better known contemporary events. Maxwell was, by all accounts, a fairly modest man. Preserving much of his innovative laboratory would be a highly appropriate memorial for this great man and also a tribute to his remarkable successors.

*Response*  
  
Comments duly noted.  
Careful consideration and attention has been given towards the restoration of the site's built heritage. Works to the Cavendish Laboratory as part of the Student Services Centre project, which also impacted the Arts School, were carefully assessed by the City Council and Historic England as part of application 15/0779/LBC and were approved in 2015.

*Action*  
  
No further action.

## Paragraph 1.4.1

### Representation(s)

**30710** Sam Smith [5762]

### Nature Appearance Soundness Tests

**Object Not Specified None**

#### Summary:

{this may be a duplicate submission. Disregard if the submission a few minutes ago succeeded -- I got an error message}

A missing "key issue" is the old Cavendish Laboratory, and the proposals for the destruction of that piece of heritage, in conflict with the statement of intent in 1.1.1

The old-Cavendish Laboratory is a key piece of scientific history, and the external view should be retained, within reason, and the style maintained, even as there is large scale refurbishment for a new purpose, in line with 1.4.1.b,c,d,e,f

### Response

**Comments duly noted.**

**The key issues section is not considered appropriate to single out individual buildings. There are many other significant buildings of heritage significance that would then need to be listed out herein, this is done later in the document at a more appropriate level of detail.**

### Action

**No further action.**

## Paragraph 1.5.1

### Representation(s)

**30688** Mr Ekbal Hussain [5738]

### Nature Appearance Soundness Tests

**Object Not Specified None**

#### Summary:

You claim that this work is to "safeguard the architectural, urban, historic, cultural and archaeological importance of the site". However I have just read that the plans include demolition of the Mond laboratory and other parts of the Cavendish labs. I urge you to reconsider. Being an alumni of Cambridge and a student of the physical sciences I cannot stress the historical importance of these labs both to Cambridge and the sciences as a whole. The Cavendish section could be better preserved by turning it into a museum to showcase the brilliant historical achievements by the scientists who worked there.

### Response

**Comments duly noted.**

**Careful consideration and attention has been given towards the restoration of the site's built heritage. Works to the Cavendish Laboratory as part of the Student Services Centre project, which also impacted the Arts School, were carefully assessed by the City Council and Historic England as part of application 15/0779/LBC and were approved in 2015. Only the Mond annexe is scheduled for demolition.**

### Action

**No further action.**

## 2. Planning Context

### Paragraph 2.2.3

#### Representation(s)

**30636** Historic England (David Grech) [1787]

#### Nature Appearance Soundness Tests

Comment Not Specified None

*Agent: Historic England (David Grech) [1787]*

#### Summary:

When making reference to the fact that English Heritage has engaged with the preparation of the Masterplan, it would helpful to note in brackets that English Heritage is now known as Historic England (so as to avoid confusion with the English Heritage Trust).

#### Response

Comments duly noted. Delete all references to "English Heritage" and replace with "Historic England". [No reference to English Heritage is made in the SPD].

#### Action

Delete all references to "English Heritage" and replace with "Historic England".

### Paragraph 2.3.3

#### Representation(s)

**30693** Prof. John Roulston [5747]

#### Nature Appearance Soundness Tests

Object Not Specified None

#### Summary:

Loses important connection with the Cavendish Lab history. The importance in experimental physics is extreme and the proposal demolishes a large part of this venerable building. It is important to preserve the Lab, designed by James Clerk Maxwell. The proposal ignores the historical significance of the Cavendish lab. WE need to preserve history as an inspiration and challenge for future generations.

#### Response

Comments duly noted.

Careful consideration and attention has been given towards the restoration of the site's built heritage. Works to the Cavendish Laboratory as part of the Student Services Centre project, which also impacted the Arts School, were carefully assessed by the City Council and Historic England as part of application 15/0779/LBC and were approved in 2015.

#### Action

No further action.

### 3. Vision and Objectives

#### Paragraph 3.1.1

##### Representation(s)

**30737** Dr Shane Lawrence [5783]

##### Nature Appearance Soundness Tests

Comment Not Specified None

##### Summary:

Ensure the Arts School Building is retained and restored and renovated both inside and out as an important historical artefact and area of the University and science activity of the 20th century.  
Ensure that if the demolition of the Examination Hall Building is necessary that the linkage should be retained and the engineering undertaken very carefully.  
The style nature and height of the new Student Services Building should be specified in more detail  
The Mond Building in general should be retained for offices.  
The Shell Building should be retained for laboratories and offices while only the Goldsmiths Laboratory should be demolished for more space at the south end.  
The entrance through the renovated and restored Arts School should be undertaken very carefully and the entrance from Pembroke Street should also be considered very carefully both in keeping with the frontage and the engineering aspects.  
The future of housing the entire Science Library Collection should be considered from the beginning of the process either in a new purpose built building or in a conversion of an existing building within the area or another area possibly West Cambridge.

##### Response

Comments duly noted.

-Careful consideration and attention has been given towards the restoration of the site's built heritage.  
Only the Mond annexe is scheduled for demolition, as agreed via application 15/0777/FUL & 15/0779/LBC (the second application which consented to the new entrance through the Arts School);  
-Plans to house the entire Science Library Collection altogether are under consideration.

##### Action

No further action.

#### Objective 1: To capture the history and tradition of the Site

##### Representation(s)

**30694** Rabbi Jeffrey Newman [5748]

##### Nature Appearance Soundness Tests

Object Not Specified None

##### Summary:

If the university truly wishes to 'create a window into the site's history' they must do it without destroying its most important buildings.

**30736** Dr Shane Lawrence [5783]

Object Not Specified None

##### Summary:

A new purpose built building would be the ideal to hold the complete holdings of the Science Library either on the completed New Museums Site or even perhaps more ideally on a new site perhaps on the West Cambridge area of the University.

##### Response

Comments duly noted.

Plans to house the entire Science Library Collection altogether are under consideration.

##### Action

No further action.



Objective 1: To capture the history and tradition of the Site

**Representation(s)****30696** John Sinton [5749]**Nature****Object****Appearance****Not Specified****Soundness Tests****None****Summary:**

The need for the university to promote 'working and learning' is not disputed. To do otherwise is to destroy the university.

But to attempt that by the effective destruction of buildings with tremendous historical and scientific significance - and demolition of the ground level facades effectively destroys them - is no less certainly destroying the university, as any meaningful connections to the past are shamelessly discarded.

**Response**

Comments duly noted.

Careful consideration and attention has and will be given towards the alteration of the site's built heritage. Detailed consultation is undertaken with the Council, Historic England and others before any proposals are put forward which could impact on assets of heritage significance, either via their use of design.

**Action**

No further action.

**Objective 3: To retain heritage and quality buildings that contribute to the site and its surroundings****Representation(s)****30637** Historic England (David Grech) [1787]**Nature****Comment****Appearance****Not Specified****Soundness Tests****None**

Agent: Historic England (David Grech) [1787]

**Summary:**

I would suggest that a final sentence is added to this paragraph along the lines of 'Some core buildings are of significance and those of greatest significance will be retained.' This will then acknowledge that the Mond Building is listed and will be kept, but allow the Mond Annex (a BLI) to be removed.

**Response**

Comments duly noted. Agree to amend Objective 3 by adding an additional sentence at the end of this paragraph to read: "Some core buildings are of significance and those of greatest significance will be retained".

**Action**

Amend Objective 3 by adding an additional sentence at the end of this paragraph to read: "Some core buildings are of significance and those of greatest significance will be retained".

Objective 3: To retain heritage and quality buildings that contribute to the site and its surroundings

**Representation(s)****30731** Dr Shane Lawrence [5783]**Nature****Support****Appearance****Not Specified****Soundness Tests****None****Summary:**

A new entrance from Benets Yard is proposed through the renovated Arts School presumably to serve as a pedestrian and not vehicular access into the central square and to the Student Services Centre; in keeping with the renovated wing of the Alts School and Benets Yard presumably.

i) The intention to retain the Arts School building is admirable and observably legal as the interior especially the Science Library rooms and staircase etc are also of listed value and as an historical memory of the previous century that is the 20thC of the activities of the University and its' science heritage.

**Response****Comments duly noted.****Action****No further action.****Objective 4: To provide a new spatial structure with links to the city****Representation(s)****30698** John Sinton [5749]**Nature****Object****Appearance****Not Specified****Soundness Tests****None****Summary:**

There is an internationally recognized context to Cambridge. That context is largely provided by the buildings. Absent the buildings and connection to the past, there is no particular reason for the current and future university to be there at all. There are other rivers, other opportunities to create a 'new town' for the university if that is what is sought.

But to destroy these very connections in order to create a piecemeal 'new town' is to copy everything that was wrong with the original Arndale shopping development. How could we make the same mistakes again?

**Response****Comments duly noted.**

**Careful consideration and attention has and will continue to be given towards any alteration or restoration of the site's built heritage.**

**Action****No further action.**

## 4. The Existing Site

### Paragraph 4.2.5

#### Representation(s)

30697 Dr Michael Pryce [5750]

#### Nature

Object

#### Appearance

Not Specified

#### Soundness Tests

None

#### Summary:

The development of the Cavendish labs, or rather their partial demolition, would destroy the space in which much important science was carried out.

As the Cavendish was built in recognition of the importance of the place in which science was carried out to its success, the loss of the building in part or in whole would mean historians and the wider cultural community lose the ability to understand how science of such high calibre was truly done.

#### Response

Comments duly noted.

Careful consideration and attention has been given towards the restoration of the site's built heritage. Works to the Cavendish Laboratory as part of the Student Services Centre project, which also impacted the Arts School, were carefully assessed by the City Council and Historic England as part of application 15/0779/LBC and were approved in 2015.

#### Action

No further action.

### Paragraph 4.2.6

#### Representation(s)

30638 Historic England (David Grech) [1787]

#### Nature

Comment

#### Appearance

Not Specified

#### Soundness Tests

None

Agent: Historic England (David Grech) [1787]

#### Summary:

For the sake of clarity (and to avoid confusion with the current Botanic Gardens) I suggest the final part of the sentence reads 'a good deal of the former Botanic Garden remained undeveloped.'

#### Response

Comments duly noted. Agree to amend paragraph 4.2.6 to add the word "former" after the words "...of the..." in the final sentence.

#### Action

Amend paragraph 4.2.6 to add the word "former" after the words "...of the..." in the final sentence.

## Paragraph b. Listed Buildings, 4.3.7

### Representation(s)

**30706** Dr Ian Patterson [1997]

### Nature Appearance Soundness Tests

**Comment** **Not Specified** **None**

#### Summary:

The Mond and Cavendish buildings are both of too great an historical significance to be partly destroyed.

### Response

Comments duly noted.

Careful consideration and attention has been given towards the restoration of the site's built heritage. Works to the Cavendish Laboratory as part of the Student Services Centre project, which also impacted the Arts School, were carefully assessed by the City Council and Historic England as part of application 15/0779/LBC and were approved in 2015. Only the Mond annexe is scheduled for demolition.

### Action

No further action.

### Representation(s)

**30728** Mr David Gibbon [5776]

### Nature Appearance Soundness Tests

**Object** **Not Specified** **None**

#### Summary:

The Cavendish Building is of international significance and its Grade II listing understates its intrinsic importance. That it was the first of its kind and that it was largely designed by James Clerk Maxwell put it into a category deserving of far greater respect than the scant regard paid to it by this scheme.

### Response

Comments duly noted.

Careful consideration and attention has been given towards the restoration of the site's built heritage. Works to the Cavendish Laboratory as part of the Student Services Centre project, which also impacted the Arts School, were carefully assessed by the City Council and Historic England as part of application 15/0779/LBC and were approved in 2015.

### Action

No further action.

## Paragraph e. Other Buildings of Heritage Interest, 4.3.12

### Representation(s)

**30732** Dr Shane Lawrence [5783]

### Nature Appearance Soundness Tests

**Comment** **Not Specified** **None**

#### Summary:

ii) The need to demolish the Examination Hall as it becomes proximal that is very close to the new Zoology building that is the Attenborough Building as it approaches completion is apparent although this action should not be hurried. The reason for this is that the Examination Building is connected with the Arts School building and the separation of them by means of demolition would be a serious operation which should be very carefully planned beforehand.

### Response

Comments duly noted.

The demolition of the Examination Hall has been approved via approval of application 15/0777/FUL

### Action

No further action.

### Paragraph f. Significance, 4.3.13

<i>Representation(s)</i>		<i>Nature</i>	<i>Appearance</i>	<i>Soundness Tests</i>
30639	Historic England (David Grech) [1787]	Comment	Not Specified	None
<i>Agent: Historic England (David Grech) [1787]</i>				
<i>Summary:</i>				
Final sentence. Here I suggest adding 'and Augustinian Friary' after King's Ditch at the end of the paragraph.				

#### Response

Comments duly noted. Agree to amend paragraph 4.3.13 to add the words "and Augustinian Friary" after the words "King's Ditch".

#### Action

Agree to amend paragraph 4.3.13 to add the words "and Augustinian Friary" after the words "King's Ditch".

### Paragraph f. Significance, Plan No.7: Heritage Significance

<i>Representation(s)</i>		<i>Nature</i>	<i>Appearance</i>	<i>Soundness Tests</i>
30711	Dr Richard Staley [5763]	Object	Not Specified	None
<i>Summary:</i>				
In this plan only part of the original Cavendish Laboratory is designated as being of Very High heritage significance, and no grounds are given for distinguishing the front of the building from the side wing, which held many significant teaching and research laboratories but is here designated as being only of High significance. The proposal to create a new entry to the site through the side wing does not reflect the considerable value of retaining the integrity of the original laboratory building. The purpose built combination of teaching and research spaces was novel, and central to the success of the laboratory.				

#### Response

Comments duly noted.  
The significance of the Cavendish Laboratory has been carefully considered together with Historic England and the City Council and is considered appropriate.

#### Action

No further action.

<i>Representation(s)</i>		<i>Nature</i>	<i>Appearance</i>	<i>Soundness Tests</i>
30690	Ms Vicky Morgan [5741]	Object	Not Specified	None
<i>Summary:</i>				
The Mond laboratory should be reassigned to high importance. As an alumna of the university and a Cambridgeshire resident, I know that this is a much-loved building rich with symbolism and the history of the great science on this site. The complex of the Cavendish and the two Mond buildings is iconic and the consultants appear to have made their assessment with no understanding of the importance of this place in the history of ideas. Surely this is what the heritage of Cambridge is all about? The modernist parts of the complex are also attractive.				

#### Response

Comments duly noted.  
Only the Mond annexe (a Building of Local Interest) is scheduled for demolition as agreed under application 15/0997/LBC. The removal of which will enhance the setting of both the Mond Building and the Cavendish Laboratory.

#### Action

No further action.

Paragraph 4.6.1

Representation(s)		Nature	Appearance	Soundness Tests
30640	Historic England (David Grech) [1787]	Comment	Not Specified	None

Agent: Historic England (David Grech) [1787]

Summary:

This states that access to the site is currently very poor. However, I question this and would suggest that what is very poor is the permeability of the site. If you compare plan 10, which illustrates the current access points, with map 13, which shows the proposed access points there is no change. I therefore suggest that the opening sentence of paragraph 4.6.1 reads: 'Access into the site is currently poor and permeability through the site is very poor, as highlighted by Plan 10.'

Response

Comments duly noted. For many users, especially cyclists and pedestrians, access is very poor and often the subject of conflict with vehicles, construction traffic, etc.. Permeability is very poor too, however this is fully recognised and the parameters for change address this in a significant fashion. No change proposed.

Action

No further action.





5. Parameters For Change

Paragraph a. The Urban Block, 5.1.2

Representation(s)	Nature	Appearance	Soundness Tests
30700 Charlotte Schoonman [5751]	Object	Not Specified	None

*Summary:*  
The closed character of the NMS frontage at Pembroke Street contributes to the awe-inspiring beauty of this area of Cambridge, providing a concrete representation of the part of the city regarded as a bastion devoted to study and the pursuit of knowledge. While I agree that the congestion in the area is undesirable, I disagree that the solution would be to open retail outlets. Turning this part of Pembroke St into yet another retail area (of which Cambridge knows no lack) diminishes the non-commercial, academic character of this unique street.

*Response*  
Comments duly noted. The introduction of some specialist retail units at ground level along Pembroke Street is only an option as noted in paragraph 5.1.12, in any event permission for change of use would likely be necessary and would enable detailed assessment of whether such uses were appropriate.

*Action*  
No further action.

Paragraph b. Urban Connectivity, 5.1.5

Representation(s)	Nature	Appearance	Soundness Tests
30721 Universities Superannuation Scheme Ltd (USS) [230]	Object	Not Specified	None

*Agent: Deloitte (Ms Julia Chowings) [5530]*  
*Summary:*  
Paragraph 5.1.5 references the vehicular entrance and exit to the Grand Arcade car park. The SPD states the intention is, if possible, for increased areas of pavement to improve the safety of pedestrians but it is not intended for Corn Exchange Street to become the principal place of arrival for pedestrians or for it to be more than the service road. USS support enhanced connectivity only if there are no future impacts or restrictions for the car parks / Grand Arcade and that access is not restricted as a result of the changes.

*Response*  
Concerns duly noted. Early consultation with neighbouring and, or potentially concerned site users, to be consulted prior to formal planning application submission to overcome cited concerns.

*Action*  
No further action.

## Paragraph b. Urban Connectivity, 5.1.11

### Representation(s)

**30632** Caitlin Alvey [5723]

### Nature Appearance Soundness Tests

**Comment** **Not Specified** **None**

#### Summary:

Yes to making pedestrians a priority. Please don't forget people on bikes. You must do something to ease the amount of traffic, or cars will still dominate the space, no matter how pretty it looks.

### Response

Concerns duly noted. Section 5.1 of the SPD sets out strategies to address issues of access and traffic management. Equally section 5.2 sets out improvements to the public realm to create significant new areas of open space will be achieved.

### Action

No further action.

### Representation(s)

**30722** Universities Superannuation Scheme Ltd (USS) [230]

### Nature Appearance Soundness Tests

**Object** **Not Specified** **None**

**Agent:** Deloitte (Ms Julia Chowings) [5530]

#### Summary:

Paragraph 5.1.11 proposes changes to the design of the paving and the introduction of trees and a system of controlling traffic queues for the car park during busy periods. Moving forward, USS would resist restrictions on the car parks that will impact upon their operational requirements therefore request that we discuss proposals and potential impacts as they emerge to ensure that USS' requirements are taken into consideration.

USS would resist future restrictions on current operations as a result of any proposals. Due to the proposals set out above and in light of the proximity of Grand Arcade and the exit of car parks on to Corn Exchange Street, USS seek to engage with Cambridge City Council to further understand the aspirations for the public realm improvements and any alterations to highways as proposals develop in the future.

### Response

Concerns duly noted. Works to the east side of the site on Corn Exchange Street are nearing completion, however early consultation with neighbouring and, or potentially concerned site users, to be consulted prior to any future formal planning application submission to overcome cited concerns.

### Action

No further action.

## Paragraph b. Urban Connectivity, 5.1.12

<i>Representation(s)</i>		<i>Nature</i>	<i>Appearance</i>	<i>Soundness Tests</i>
30641	Historic England (David Grech) [1787]	Comment	Not Specified	None
<i>Agent: Historic England (David Grech) [1787]</i>				
<i>Summary:</i>				
As part of testing the appropriateness of introducing small specialist retail outlets into the Pembroke Street frontage, I suggest that the implication for signage and advertising would need to be carefully examined (though would not wish to prejudice the impact).				

*Response*

Comments duly noted. This matter would be given due consideration at the planning application stage.

*Action*

No further action.

=====

## Paragraph c. Site Permeability, 5.1.13

<i>Representation(s)</i>		<i>Nature</i>	<i>Appearance</i>	<i>Soundness Tests</i>
30705	Rachel Engler [5760]	Comment	Not Specified	None
<i>Summary:</i>				
I do not support new entrances on Pembroke Street and Free School Lane. The current entrances are adequate for pedestrians and cyclists. Both streets are part of the few visually appealing streets left in the city centre. The proposal that new entrances would be sensitive to historical surroundings is vague and easily ignored in today's planning environment. The B'enet Street and Corn Exchange entrances could be widened and made the main public entrances. There are more pedestrians in that area anyway. The Whipple Museum main entrance could be in the new courtyard which is proposed.				

*Response*

Comments duly noted.

Section 5.1 explains the objectives and constraints to the Pembroke Street access. It is made clear that the entrances are not adequate nor do they support the creation of a better connected, more legible environment for both public and university use. The other entrances are equally part of the wider site access arrangements that either have, or will be, considered in detail. Careful consideration of the impact on building fabric and heritage assets will be undertaken before any works requiring planning permission are granted.

*Action*

No further action.

=====

**Representation(s)****30730** Robyn Arianrhod [5778]**Nature****Object****Appearance****Not Specified****Soundness Tests****None****Summary:**

This iconic laboratory - whose first director and co-designer was James Clerk Maxwell, one of the greatest (and most under-celebrated) geniuses in the history of science - is important to science lovers throughout the world. I would like to see it as a museum celebrating the Laboratory's incredible achievements, but the proposed development would impact not only the front facade but also the original layout and design of what is one of the first purpose-built academic laboratories in the world. Also, perhaps the exterior of the "plain" back wall can be used for an additional display celebrating the Laboratory's proud and colourful history.

**Response****Comments duly noted.**

**Works to the Cavendish Laboratory as part of the Student Services Centre project, which also impacted the Arts School, were carefully assessed by the City Council and Historic England as part of application 15/0779/LBC and were approved in 2015.**

**Action****No further action.****Representation(s)****30729** Basil Mahon [5777]**Nature****Object****Appearance****Not Specified****Soundness Tests****None****Summary:**

I agree with Simon Schaffer that "there is, perhaps, no other scientific building in this country as important as the original Cavendish".

Not only was the building designed by one of the greatest scientists who ever lived, the Cavendish's founding director James Clerk Maxwell, it has been the site of some of the most important scientific discoveries, including the electron and the structure of DNA. Among the world-leading scientists who have worked there are Cavendish's second director Lord Rayleigh, its third director J.J. Thomson, and Ernest Rutherford.

It is a historic and irreplaceable building, which should be lovingly restored, not mutilated.

**Response****Comments duly noted.**

**Careful consideration and attention has been given towards the restoration of the site's built heritage.**

**Works to the Cavendish Laboratory as part of the Student Services Centre project, which also impacted the Arts School, were carefully assessed by the City Council and Historic England as part of application 15/0779/LBC and were approved in 2015. Only the Mond annexe is scheduled for demolition.**

**Action****No further action.**

**Representation(s)****Nature****Appearance****Soundness Tests****30726** Prof. Hasok Chang [5758]**Object****Not Specified****None****Summary:**

I would be very concerned about any plans to demolish any part of the original structure of the Old Cavendish Laboratory. It would be a great irony if an effort to make the New Museums Site more accessible and better known to the public ended up damaging THE most iconic and important historical site in the whole site.

**Response****Comments duly noted.**

**Works to the Cavendish Laboratory as part of the Student Services Centre project, which also impacted the Arts School, were carefully assessed by the City Council and Historic England as part of application 15/0779/LBC and were approved in 2015.**

**Action****No further action.****Representation(s)****Nature****Appearance****Soundness Tests****30725** Paul Joseph Moore [5775]**Object****Not Specified****None****Summary:**

Clark Maxwell's contribution to modern science cannot be overstated and his lab original design, here under threat, was a groundbreaking departure whose integrity MUST be respected

The epoch-making discoveries made here are widely acknowledged as part Maxwell's legacy, in addition to his own immense contributions. make this a uniquely iconic site.

Maxwell's birthplace in Edinburgh and his home in Galloway were both long neglected but are now being protected and restored. Please see sense and respect his legacy in your own establishment. Would this be considered if the lab were Newton's or Einstein's. Maxwell deserves no less respect.

I feel the use of the word "vital" on the side of the proposed works betrays a bias towards the work and I'd say instead it is VITAL that Maxwell's legacy be respected.

**Response****Comments duly noted.**

**Works to the Cavendish Laboratory as part of the Student Services Centre project, which also impacted the Arts School, were carefully assessed by the City Council and Historic England as part of application 15/0779/LBC and were approved in 2015.**

**Action****No further action.**

**Representation(s)****30724 Dr Isobel Falconer [5770]****Nature****Object****Appearance****Not Specified****Soundness Tests****None****Summary:**

The paragraph overlooks the heritage value of this wing of the Cavendish Laboratory, where J J Thomson discovered the electron and the isotopes of neon, Rutherford used alpha particles to probe the structure of the atomic nucleus, and Chadwick discovered the neutron. The new entrance will destroy internal arrangements, including Rutherford's personal laboratory and pre-empt plans for the restoration of the Old Cavendish as a science heritage centre and visitor attraction.

**Response****Comments duly noted.**

**Works to the Cavendish Laboratory as part of the Student Services Centre project, which also impacted the Arts School, were carefully assessed by the City Council and Historic England as part of application 15/0779/LBC and were approved in 2015.**

**Action****No further action.****Representation(s)****30723 James Clerk Maxwell Foundation (David Forfar) [5772]****Nature****Object****Appearance****Not Specified****Soundness Tests****None****Summary:**

This paragraph is highly misleading. Blank, plain wall is not at all blank.  
The proposed new entrance would actually involve the destruction of two pairs of windows on opposite sides of the building, together with the historically important site of (Nobel Laureate) Lord Rutherford's personal laboratory! The Old Cavendish is a building of great historical significance, but this is not reflected in the description. No destruction to the fabric of this building must be permitted, and consideration should be given to raising its grade. It is incumbent upon both the City and the University to ensure that it is appropriately refurbished to the highest standards and put to a good, scientifically related, use.  
To destroy any part of its core fabric may only be described as cultural vandalism, and would also appear to contravene Policy 61 and Policy 62 of the Cambridge Local Plan 2014.

**Response****Comments duly noted.**

**Careful consideration and attention has been given towards the restoration of the site's built heritage. Works to the Cavendish Laboratory as part of the Student Services Centre project, which also impacted the Arts School, were carefully assessed by the City Council and Historic England as part of application 15/0779/LBC and were approved in 2015. Only the Mond annexe is scheduled for demolition.**

**Action****No further action.**



**Representation(s)****Nature****Appearance****Soundness Tests****30720** The Rt Hon Alex Fergusson [5774]**Object****Not Specified****None****Summary:**

I wish to strongly object to the plans to partially demolish the Cavendish Laboratory. As someone who has tried to champion the life, works and achievements of James Clerk Maxwell, whose family estate lies within my Scottish Parliamentary constituency of Galloway and West Dumfries within which he is buried, I believe that he is one of the most understated geniuses in our history. He designed the Cavendish Laboratory and was its first head, and it was under his leadership that the Cavendish spawned what a recent article called 'one of the greatest crop of experimental scientists the world has known'.

This building simply must be preserved. As others have pointed out, "Elsewhere historically insignificant buildings are being kept for no good reason, yet there is perhaps no other scientific building in this country as important as the original Cavendish, perhaps no laboratory as aesthetically pleasing as the Mond. If the university truly wishes to 'create a window into the site's history' they must do it without destroying its most important buildings".

I whole-heartedly concur.

**Response**

**Comments duly noted.**

**Careful consideration and attention has been given towards the restoration of the site's built heritage. Works to the Cavendish Laboratory as part of the Student Services Centre project, which also impacted the Arts School, were carefully assessed by the City Council and Historic England as part of application 15/0779/LBC and were approved in 2015. Only the Mond annexe is scheduled for demolition.**

**Action**

**No further action.**

**Representation(s)****Nature****Appearance****Soundness Tests****30719** Dr John Reid [5773]**Object****Not Specified****None****Summary:**

The proposals as submitted, though formulated with good intentions, will clearly have detrimental unintended consequences. The original appearance of the building will be altered, as well as incurring the loss of Rutherford's personal laboratory. They should be refused in their present form and returned for further consideration.

The Cavendish Laboratory has iconic status for science and society at large. In a way it is unfortunate for the University and the City that the Cavendish has this iconic status, for with it comes the obligation to consult very widely on proposed changes both internal and external before plans are submitted. This has clearly not been done. The University feels it could do more with the Cavendish Laboratory. Excellent. One example they should study is the sympathetic restoration and re-purposing of their historic laboratories by the University of Coimbra, celebrating 750 years of history. More sympathetic development of the Cavendish than the submitted plans is possible and should be investigated urgently before the tabled, well-meaning but detrimental, proposals are acted on.

**Response**

**Comments duly noted.**

**Careful consideration and attention has been given towards the restoration of the site's built heritage. Works to the Cavendish Laboratory as part of the Student Services Centre project, which also impacted the Arts School, were carefully assessed by the City Council and Historic England as part of application 15/0779/LBC and were approved in 2015.**

**Action**

**No further action.**

**Representation(s)****30718** Dr Adam Hart-Davis [5769]**Nature****Object****Appearance****Not Specified****Soundness Tests****None****Summary:**

This sounds an absolute disaster. For every scientist in the world the Cavendish Lab represents the most important building in the entire history of science. More Nobel Prizes have come to the Cavendish than to anywhere else on earth.

You cannot destroy this building. It must be preserved.

**Response**

**Comments duly noted.**

**Careful consideration and attention has been given towards the restoration of the site's built heritage. Works to the Cavendish Laboratory as part of the Student Services Centre project, which also impacted the Arts School, were carefully assessed by the City Council and Historic England as part of application 15/0779/LBC and were approved in 2015.**

**Action**

**No further action.**

**Representation(s)****30717** Mr Dick Middleton [5754]**Nature****Object****Appearance****Not Specified****Soundness Tests****None****Summary:**

I learned with incredulity that as part of the above development you are planning significant alterations to the Cavendish Laboratory.

This laboratory building is of extraordinary historic value in the context of modern scientific development and progress. It was designed by the somewhat unknown mathematical genius James Clerk Maxwell. Maxwell is man whose insights laid the foundation for all modern physics including radio, x-rays, light, radio astronomy, quantum physics and so on. Many Nobel Laureates (29 according to the university website) are from the Cavendish laboratory and benefited from Maxwell's work - Rutherford, Crick & Watson (DNA) to name just the best known.

Maxwell was a forward thinker and knew the importance of practical science. His design for the Cavendish Laboratory was revolutionary and was instrumental in the development of many of the most important 20th century scientists. It is a truly unique and historic building. Possibly one of the most important buildings in Cambridge. It is surely part of the heritage you are trying to preserve.

Please, I strongly object to your plan to develop this building.

**Response**

**Comments duly noted.**

**Careful consideration and attention has been given towards the restoration of the site's built heritage. Works to the Cavendish Laboratory as part of the Student Services Centre project, which also impacted the Arts School, were carefully assessed by the City Council and Historic England as part of application 15/0779/LBC and were approved in 2015.**

**Action**

**No further action.**

**Representation(s)****Nature****Appearance****Soundness Tests****30716** Prof Tom McLeish [5757]**Object****Not Specified****None****Summary:**

The paragraph suggests that the proposed development merely destroys an 'almost blank and very plain back wall of the [old] Cavendish Laboratory'. It fails to say is that on the other side of this way is an ivy-covered frontage which is undoubtedly one of the most iconic scientific buildings in the world.

This is the building where the electron was discovered, the atom first split by deliberate bombardment, the structure of DNA discovered. The destruction of this building unique to the highest points of 20th century physics is unacceptable.

**Response**

**Comments duly noted.**

**Careful consideration and attention has been given towards the restoration of the site's built heritage. Works to the Cavendish Laboratory as part of the Student Services Centre project, which also impacted the Arts School, were carefully assessed by the City Council and Historic England as part of application 15/0779/LBC and were approved in 2015.**

**Action**

**No further action.**

**Representation(s)****Nature****Appearance****Soundness Tests****30715** The Rt Hon Patrick Jenkin [5767]**Object****Not Specified****None****Summary:**

I wish to object to the proposals to redevelop part of the original Cavendish Laboratory designed by James Clerk Maxwell.

The Applicants do not appear to recognise that this building is of great historic and scientific significance. Its design and construction in the 19th Century marked a major turning point in the development of science in the world, when scientific research became a matter for the hard, sequential, logical search for provable evidence. James Clerk Maxwell was one of the most significant leaders of this revolution in science.

My interest is that my Great-Grandfather, Prof HC Fleeming Jenkin, the first Professor of Science at Edinburgh, was a close associate of and collaborator with James Clerk Maxwell; their joint work on defining the ohm, for instance, was largely undertaken in this laboratory.

If those who are planning the modernisation of this part of Cambridge University were aware of the truly historic significance of this original Cavendish Laboratory, I feel sure that their otherwise admirable proposals could be largely achieved while at the same time preserving the essential character of the entirety of this original Cavendish Laboratory.

**Response**

**Comments duly noted.**

**Careful consideration and attention has been given towards the restoration of the site's built heritage. Works to the Cavendish Laboratory as part of the Student Services Centre project, which also impacted the Arts School, were carefully assessed by the City Council and Historic England as part of application 15/0779/LBC and were approved in 2015.**

**Action**

**No further action.**

**Representation(s)****30714** Paul Davey [5768]**Nature****Object****Appearance****Not Specified****Soundness Tests****None****Summary:**

As I understand it, the plan is to go through the ground floor of the Cavendish Lab. This is poor treatment of such an historic building. As regards its listed status, although the back wall is "very plain" and might not be a great loss, the opposite wall where the route would emerge is far from being plain.

**Response**

Comments duly noted.

Careful consideration and attention has been given towards the restoration of the site's built heritage. Works to the Cavendish Laboratory as part of the Student Services Centre project, which also impacted the Arts School, were carefully assessed by the City Council and Historic England as part of application 15/0779/LBC and were approved in 2015.

**Action**

No further action.

**Representation(s)****30713** Prof Wilson Poon [5766]**Nature****Object****Appearance****Not Specified****Soundness Tests****None****Summary:**

Paragraph suggests that the development merely destroys an 'almost blank and very plain back wall of the [old] Cavendish Laboratory'. But on the other side is an ivy-covered frontage that is undoubtedly one of the world's most iconic scientific buildings. It is unclear how nearby buildings will be affected, notably the Mond Laboratory. Destroying this heritage is vandalism on a par with what ISIS is doing elsewhere right now. As a senior academic, I understand the need for space. But it should be possible to satisfy these needs without irreparably damaging the integrity of one of the top 10 historic scientific sites in the world.

**Response**

Comments duly noted.

Careful consideration and attention has been given towards the restoration of the site's built heritage. Works to the Cavendish Laboratory as part of the Student Services Centre project, which also impacted the Arts School, were carefully assessed by the City Council and Historic England as part of application 15/0779/LBC and were approved in 2015. Only the Mond annexe is scheduled for demolition.

**Action**

No further action.

**Representation(s)****30703** Dr Boris Jardine [5759]**Nature****Object****Appearance****Not Specified****Soundness Tests****None****Summary:**

Alterations to the Old Cavendish should be kept to an absolute minimum. The building is one of the very first purpose-built scientific laboratories in the country, and as such is amongst the most important scientific buildings in the world. There is a huge body of historical scholarship that backs up this statement. The benefit of opening the site up at the north end is not at all clear -- one stated intention is to 'reveal buildings of interest', but this cannot be justified if the main 'building of interest' on the entire site is disfigured.

**Response**

Comments duly noted.

Careful consideration and attention has been given towards the restoration of the site's built heritage. Works to the Cavendish Laboratory as part of the Student Services Centre project, which also impacted the Arts School, were carefully assessed by the City Council and Historic England as part of application 15/0779/LBC and were approved in 2015. Only the Mond annexe is scheduled for demolition.

**Action**

No further action.

**Representation(s)****30691 Mr Simon Lock [5744]****Nature****Object****Appearance****Not Specified****Soundness Tests****None****Summary:**

The modification of the exterior of such a significant building as the old cavendish laboratories seems misguided. This is one of the most scientifically significant buildings in the world. It should be preserved to as great a degree as possible as a lasting monument to the achievements achieved within its walls and the systems of scientific learning established there.

**Response****Comments duly noted.**

Careful consideration and attention has been given towards the restoration of the site's built heritage. Works to the Cavendish Laboratory as part of the Student Services Centre project, which also impacted the Arts School, were carefully assessed by the City Council and Historic England as part of application 15/0779/LBC and were approved in 2015.

**Action****No further action.****Representation(s)****30702 Dr William Tobin [5756]****Nature****Support****Appearance****Not Specified****Soundness Tests****None****Summary:**

Boris Jardine's article in The Guardian alerted me to the proposal to improve pedestrian access to the Old museums Site by piercing the back wall of the oldest part of the Old Cavendish Laboratory (the East Wing).

While recognising and approving of the respect for heritage, and feeling that there is insufficient respect for scientific and industrial heritage, my belief is that we must not let the past stifle the present. So long as "existing window openings [are] sensitively adapted and possibly enlarged" this seems to me an appropriate compromise between past and present.

**Response****Comments duly noted.****Action****No further action.****Paragraph a. Public Realm within the site, 5.2.1****Representation(s)****30734 Dr Shane Lawrence [5783]****Nature****Comment****Appearance****Not Specified****Soundness Tests****None****Summary:**

iv) When it comes to the buildings in the centre of the square area the central area would be enhanced by more pedestrianisation and trees in the centre which would justify the demolition of the centrally placed Rolls Royce Laboratory.

**Response****Comments duly noted.**

Sufficient space has been catered for space across three new squares for a variety of uses.

**Action****No further action.**

## Paragraph a. Public Realm within the site, 5.2.2

### Representation(s)

30735 Dr Shane Lawrence [5783]

### Nature Appearance Soundness Tests

Comment Not Specified None

#### Summary:

v) The adequate space provided within the central area (between the Austin Building and the Attenborough Building) will not require any demolition or removal of any part of the Austin Building.

vi) The retained Mond Building fits with the increased pedestrianised space although the purpose of the reduced Mond Building is not indicated.

vii) To the south of the area it is envisaged demolishing the Shell Building and the Goldsmiths Laboratory to enable 'greening of the area around a new entranceway from Pembroke Street. Again this would contribute to the increased space between the Attenborough Building and the Old Metallurgy Building which it is intended to retain.

viii) The necessity of demolishing both the Shell Building and Goldsmiths Laboratory to attain such a space is not clear. If the size of the Attenborough Building annexe is only as is almost completed at the moment then adequate pedestrian space by the removal of the Goldsmiths Laboratory only and not the Shell Building as well would be a better and more realistic plan.

ix) The making of a new entrance through the Heycock Wing into the central area should be very carefully considered as this frontage onto Pembroke Street is also of listed nature and the engineering difficult. Could not the existing entrance from Pembroke Street be slightly altered to serve as the entrance?

### Response

Comments duly noted.

-Demolition of the Shell Building or the Goldsmiths Laboratory are required to achieve public realm as anticipated in the university master plan and which the SPD supports; and

-Comments regarding careful treatment of the Pembroke Street façade are noted and any changes to this elevation will be considered as part of any works to the 1907 building. Note these buildings are not however statutorily listed.

### Action

No further action.

### Representation(s)

30642 Historic England (David Grech) [1787]

### Nature Appearance Soundness Tests

Comment Not Specified None

Agent: Historic England (David Grech) [1787]

#### Summary:

Creating the third court would require the loss of the Mond Annexe, a building of local interest but, on balance, the benefits to enhancing the setting of both the Mond Building and the Cavendish Laboratory (and thereby better revealing the significance of these two listed buildings) would more than outweigh the harm of the loss of this BLI.'

### Response

Comments duly noted.

### Action

No further action.



**Representation(s)****30727** Prof. Hasok Chang [5758]**Nature****Object****Appearance****Not Specified****Soundness Tests****None****Summary:**

It is not clear from this description what exactly will be done to the Mond Building, but it is an important historical site, both in its own right as a pioneering laboratory and as part of the Cavendish Laboratory complex.

**Response****Comments duly noted.****The Mond Building will be retained, only the Mond annexe is scheduled for demolition.****Action****No further action.****Representation(s)****30704** Dr Boris Jardine [5759]**Nature****Object****Appearance****Not Specified****Soundness Tests****None****Summary:**

The Mond annexe was designed and built to be part of a suite which includes the Mond Laboratory. As such it cannot be considered as a separate and subordinate building. Photographs in the architectural press dating from the opening of the Mond Lab give just as much prominence to the annexe as to the tower and crocodile. There is no evidence that the architect H.C. Hughes or his contemporaries thought of the annexe as anything other than \*part\* of the suite of Mond buildings. It must be kept for the same reasons that the Mond Lab is being preserved.

**Response****Comments duly noted.****The Mond Building will be retained, only the Mond annexe is scheduled for demolition as approved under 15/0779/LBC****Action****No further action.****Representation(s)****30701** Charlotte Schoonman [5751]**Nature****Object****Appearance****Not Specified****Soundness Tests****None****Summary:**

I oppose the destruction of these historic parts of the Old Cavendish laboratory and the Mond Laboratory for the sake of 'the public realm' for the following reasons:

- 1) While I am unsure that the lack of "clarity, cohesion, and spatial identity" is a bad thing, I believe that a tasteful, spacious solution to the cluttered feel of the NMS can be found without demolishing the spaces where such incredible discoveries and scientific advances were made.
- 2) The conservation of these precious, culturally significant laboratories is more important to the public than the 'realm' their destruction might create.

**Response****Comments duly noted.**

**Careful consideration and attention has been given towards the restoration of the site's built heritage. Works to the Cavendish Laboratory as part of the Student Services Centre project, which also impacted the Arts School, were carefully assessed by the City Council and Historic England as part of application 15/0779/LBC and were approved in 2015. Only the Mond annexe is scheduled for demolition.**

**Action****No further action.**

## Paragraph a. Public Realm within the site, Plan No.15: Proposed Open Space & The Public Realm

### Representation(s)

**30644** Historic England (David Grech) [1787]

### Nature Appearance Soundness Tests

Comment Not Specified None

*Agent: Historic England (David Grech) [1787]*

#### Summary:

Identify courtyards A, B and C on this plan

### Response

Comments duly noted.

Agree to amending Plan 15 to annotate courtyards A, B and C (or First, Second and Third, respectively)

### Action

Amending Plan 15 to annotate courtyards A, B and C (or First, Second and Third, respectively)

## Paragraph a. Public Realm within the site, 5.2.5

### Representation(s)

**30643** Historic England (David Grech) [1787]

### Nature Appearance Soundness Tests

Comment Not Specified None

*Agent: Historic England (David Grech) [1787]*

#### Summary:

I recommend that signage is added to the list of items needing a consistent treatment in the design of open spaces.

### Response

Comments duly noted.

Agree to add the words "signage" in paragraph 5.3.5 after the word "materials,".

### Action

Add the words "signage" in paragraph 5.3.5 after the word "materials,".

## Paragraph a. Public Realm within the site, 5.2.6

### Representation(s)

**30645** Historic England (David Grech) [1787]

### Nature Appearance Soundness Tests

Comment Not Specified None

*Agent: Historic England (David Grech) [1787]*

#### Summary:

Whilst the introduction of underground storage for storm water attenuation may be desirable, there would first need to be a full understanding of the implications for buried archaeology.

### Response

Comments duly noted.

Any planning application to provide storm water attenuation facilities would need to provide an archaeological assessment which would be a condition of any planning application/consent.

### Action

No further action.

## Paragraph b. New Buildings, 5.3.4

### Representation(s)

**30733** Dr Shane Lawrence [5783]

### Nature Appearance Soundness Tests

Commen Not Specified None  
t

#### Summary:

iii) The exact nature of the new Student Services Building on the site of the Examinations Hall is not completely specified although it seems to be envisaged as 3 storey and of ultramodern construction and style. Therefore the retention and care of the Arts School building should be seriously considered and planned for in detail from the beginning of work on the Examinations Hall.

### Response

Comments duly noted.

The Arts School building is to be retained and incorporated into the new approved proposal for the Student Services Centre approved under 15/0777/FUL

### Action

No further action.

## Paragraph b. New Buildings, 5.3.5

### Representation(s)

**30646** Historic England (David Grech) [1787]

### Nature Appearance Soundness Tests

Commen Not Specified None  
t

*Agent: Historic England (David Grech) [1787]*

#### Summary:

When considering the impact of new buildings on the quality of external spaces through daylight and sun path modelling; this will need to include both spaces within the New Museums Site and the adjacent public realm.

### Response

Comments duly noted. Paragraph 5.3.5 does note the word "site" in reference to the public realm, hence no change to the text is considered necessary.

### Action

No further action.

## Paragraph b. New Buildings, 5.3.9

### Representation(s)

**30633** Cambridge Live (Mr Neil Jones) [5725]

### Nature Appearance Soundness Tests

Commen Not Specified None  
t

#### Summary:

Existing build creates operational difficulties for the Cambridge Corn Exchange.

### Response

Concerns duly noted however it is not for this SPD to seek to address existing operational difficulties with existing conditions. Co-ordination between the City Council and the university will be necessary as part of any new development and during construction and later operations phases.

### Action

No further action.

Paragraph a. Sustainability, 5.4.1

Representation(s)		Nature	Appearance	Soundness Tests
30699	John Sinton [5749]	Object	Not Specified	None
<p><i>Summary:</i></p> <p>It is well seen that conservation of historical assets is last on the list of sustainability considerations. Truly good plans would find a better balance of sustainability objectives.</p> <p>It is tempting to compare the planned destruction here to that at historical sites by ISIL. In their view, pressing current needs override any obligation to respect the history.</p> <p>While the comparison sounds with ISIL sounds overdone, I struggle to see a substantial difference. Cambridge believes their rational for destruction is simply even more righteous, but the end result in both cases is unnecessary and undesirable destruction.</p>				
<p><i>Response</i></p> <p>Comments duly noted.</p> <p>Careful consideration and attention has and will be been given towards the alteration of the site's built heritage. Detailed consultation is undertaken with the Council, Historic England and others before any proposals are put forward which could impact on assets of heritage significance, either via their use of design.</p>				
<p><i>Action</i></p> <p>No further action.</p>				

Paragraph 5.5.2

Representation(s)		Nature	Appearance	Soundness Tests
30634	Cambridge Live (Mr Neil Jones) [5725]	Comment	Not Specified	None
<p><i>Summary:</i></p> <p>Careful joined-up planning of large vehicular access will be required in dialogue with the Cambridge Corn Exchange and Cambridge Arts Theatre to ensure that both arts venues can continue to ensure load ins for shows.</p>				
<p><i>Response</i></p> <p>Concerns duly noted. Early consultation with neighbouring and/or potentially concerned site users would be necessary and who would be consulted prior to a formal planning application submission with the objective to overcome cited concerns.</p>				
<p><i>Action</i></p> <p>No further action.</p>				

## 6. Planning Obligation

### Paragraph 6.1

#### Representation(s)

**30712** Cambridge Live (Mr Jeremy Newton) [5765]

#### Nature Appearance Soundness Tests

Nature	Appearance	Soundness Tests
Comment	Not Specified	None

#### Summary:

The Cambridge Corn Exchange (CCE) adjoins the New Museums Site and plays a vital function for both university and city. I propose either a 'Planning Obligation' to ensure a contribution towards its capital development costs (in order to better serve both town and gown) or, preferably, an active partnership between the NMS project team and the Cambridge Live team responsible for CCE to plan an ambitious programme of shared facilities and activities.

#### Response

Comments duly noted.

Such a planning obligation would require detailed consultation and agreement likely outside the remit of this framework or at the very least via a shared building or public realm project between the university and the council. The planning obligations set out in this SPD are considered appropriate and proportionate to the impact of the development on its surrounding, notably the need for future improvements in and around the main entrance to the site off Downing Street.

#### Action

No further action.

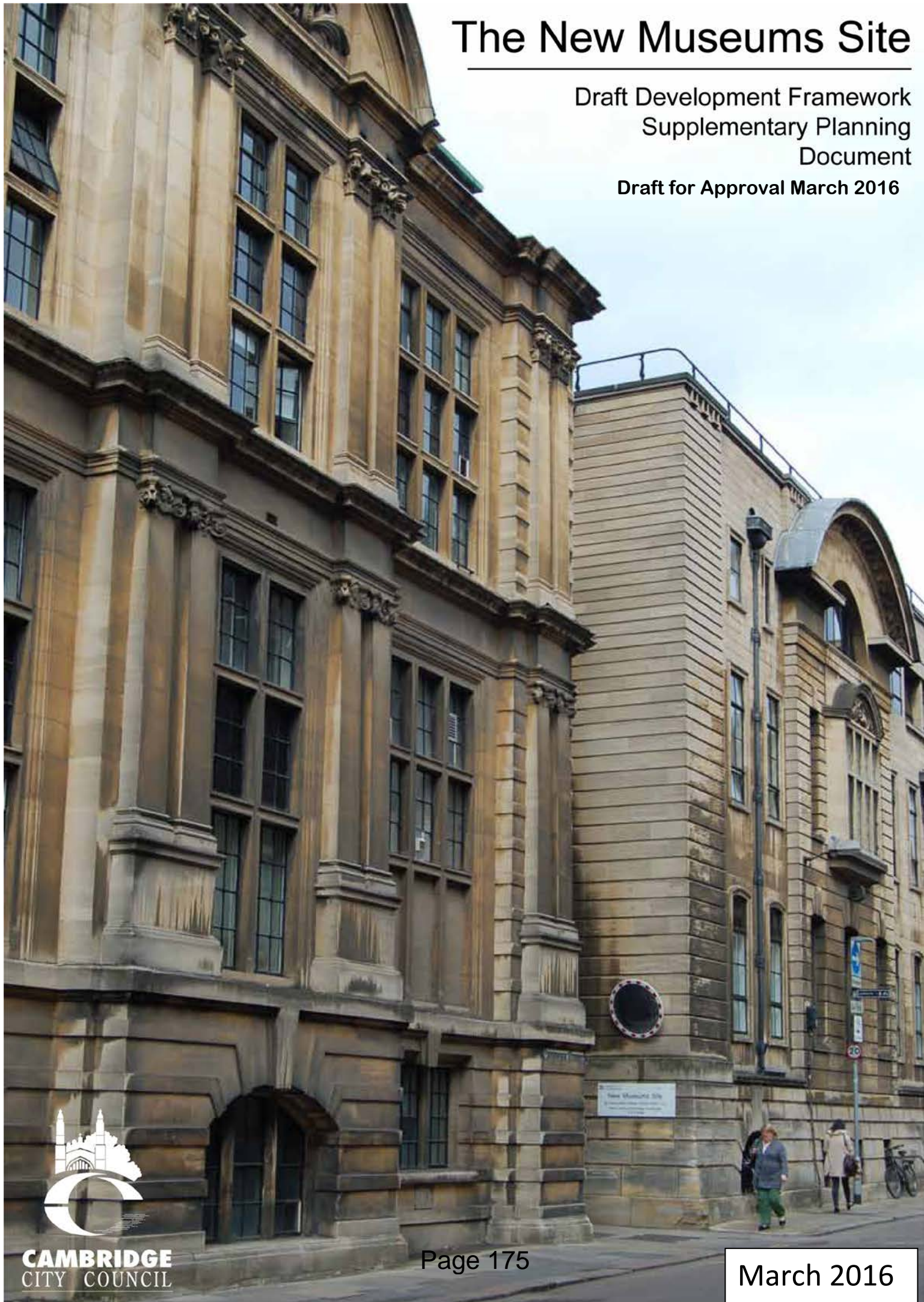
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# The New Museums Site

Draft Development Framework  
Supplementary Planning  
Document

Draft for Approval March 2016



**CAMBRIDGE**  
CITY COUNCIL

**~~DRAFT~~**  
**THE NEW MUSEUMS SITE**  
**DEVELOPMENT FRAMEWORK**  
**SUPPLEMENTARY PLANNING DOCUMENT**

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## 6.0 PLANNING OBLIGATION

### APPENDIX A - GLOSSARY OF TERMS

#### Diagrams, Plans and Figures referenced in the text

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Plan 13	Access, Entrances and Public realm outside the site boundary		

## **1. INTRODUCTION**

### **1.1 Background**

- 1.1.1 The New Museums Site (NMS), as shown in Plan 1 below, occupies a prominent location in Cambridge and has an extraordinary history as the historic centre of science in the University of Cambridge. Yet to some it is a disappointing place which, though it presents an attractive face to some surrounding streets, suffers from its piecemeal development during the first half of the 20<sup>th</sup> century, when the physical sciences grew exponentially, and from the ambitions of the post war era to address the resulting problems through comprehensive redevelopment and the imposition of an inappropriate scale. Future development on the site therefore offers an opportunity to create an improved, more coherent place and especially to improve the public realm on the site.

### **1.2 The David Attenborough Building**

- 1.2.1 Proposals for works to the David Attenborough Building, (previously known as the Arup Building), were brought forward before the preparation of this Supplementary Planning Document (SPD) as they were focussed on refurbishment and internal alterations to provide enhanced space and public access for the Zoology Museum and to provide accommodation for those organisations which are a part of the Cambridge Conservation Initiative<sup>1</sup>. The work does however include some elements which will contribute to the aspirations of this document, such as the central facilities management base, (goods in/out), and the improvement of the street scene along Corn Exchange Street. These works are underway at the time of the preparation of this document and are due for completion in late 2015/early 2016.

### **1.3 Aspirations**

- 1.3.1 This document sets out the joint aspirations of the City Council and the University for future changes through which it is hoped that a meaningful urban place, which celebrates the past, present and future, can be made.

### **1.4 Key Issues**

- 1.4.1 The key issues and opportunities on the site have been identified as follows:
- a. Enhancement of the Historic Core Conservation Area and the careful management of existing built heritage assets on and around the site
  - b. Integration of the site into the wider city centre

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<sup>1</sup> Planning application reference 13/0193/FUL

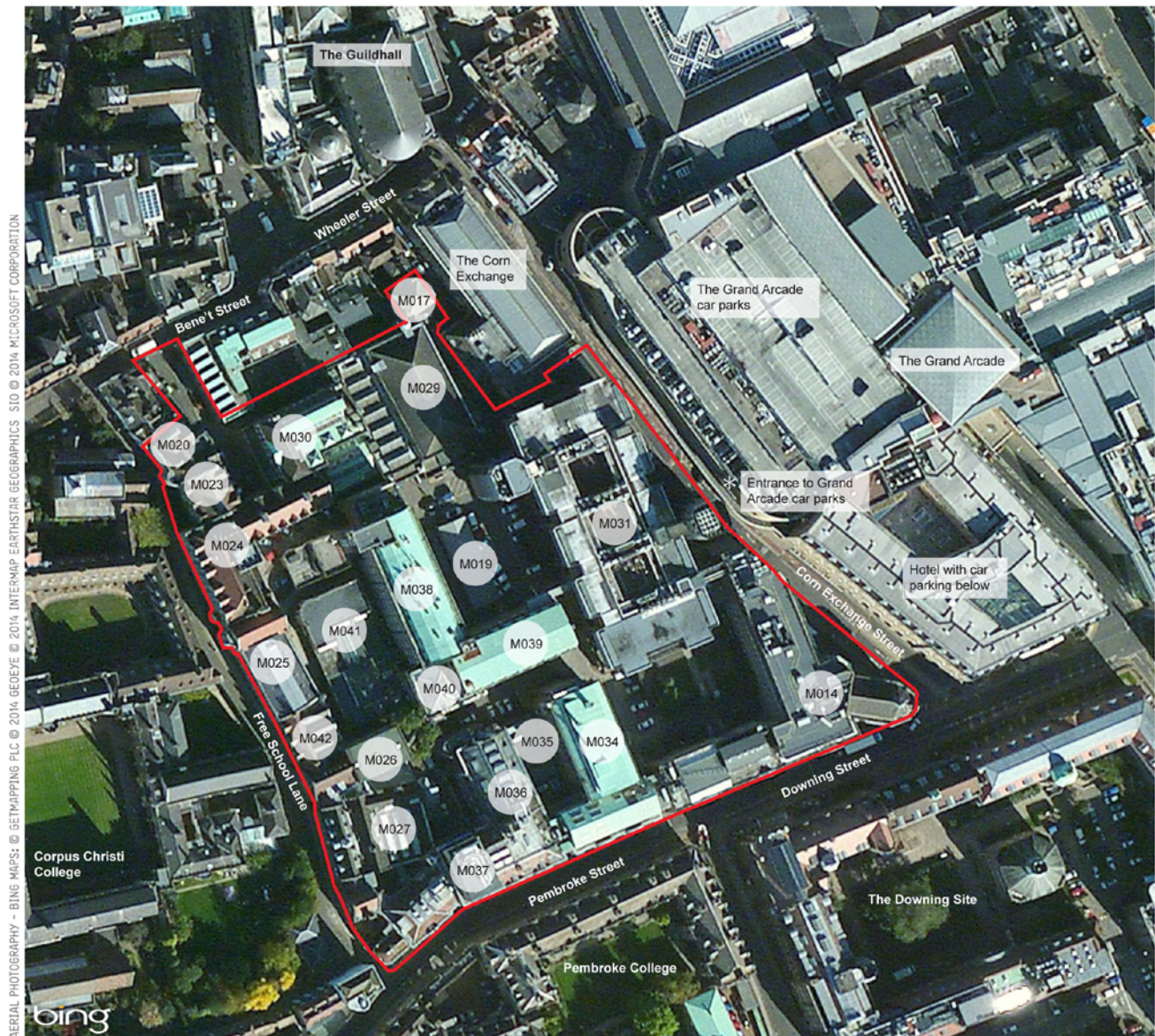
- c. Improvement of the external spaces within the site and creation of an appropriate public realm with improved permeability and public access especially to University museums and the heritage of the site
- d. Improvement of University facilities in support of its continuing academic success and its importance to the life of the city
- e. Increased environmental sustainability including better energy use, increased ecology and biodiversity and settings supporting the health and wellbeing of the site users and visitors.
- f. Reduced car parking and improved facilities for pedestrians and cyclists.



# THE NEW MUSEUMS SITE, DEVELOPMENT FRAMEWORK SPD

## THE NEW MUSEUMS SITE

### PLAN NO 1: THE SITE - EXISTING BUILDINGS



#### LEGEND

Site Extent

M014	Zoology Building	M027	Old Physical Chemistry Building	M038	Austin Building
M017	4 Parsons Court	M029	Examinations Hall	M039	Cockcroft Building
M019	Rolls Royce Laboratory	M030	Arts School	M040	Balfour Building
M020	3 Free School Lane	M031	David Attenborough Building	M041	Mond Building
M023	Old Cavendish Laboratory Rayleigh Wing	M034	Shell Building	M042	Social Anthropology Building
M024	Old Cavendish Laboratory East Wing	M035	Goldsmiths' Laboratory		
M025	Old Cavendish Laboratory	M036	Old Metallurgy Building		
M026	Phoenix Building	M037	Heycock Wing		

↑ University Reference Numbers



## 1.5 Process of Preparation

- 1.5.1 Cambridge City Council and the University of Cambridge have worked in partnership on the preparation of the SPD to ensure that any future development safeguards the architectural, urban, historic, cultural and archaeological importance of the site while providing an opportunity for positive qualitative change.

The SPD Process



## 1.6 Purpose and Scope

- 1.6.1 An SPD can be prepared to support policies and objectives found in a local plan. This SPD provides clear guidance on the City Council and University's aspirations for the New Museums site. It is a framework which will help co-ordinate and guide future redevelopment of the site in line with the Council's Local Plan policies. In particular, this SPD has been prepared to support *Policy No. 43: University faculty development* and site allocation U2 as set out in the

Cambridge Local Plan 2014: Proposed Submission (as amended<sup>2</sup>). The full policy as drafted is set out in Appendix 1 of this SPD. The SPD has been prepared in line with the requirements of the Town and County Planning (Local Planning) (England) Regulations 2012.

1.6.2 The document has been prepared jointly by both the University of Cambridge and Cambridge City Council to set out what is expected in relation to the future re-development of the site. The purpose of the SPD is threefold:

- To articulate a clear vision about the future of the New Museums Site
- To establish a framework to co-ordinate redevelopment within the site and the immediate public realm to the site and to help guide decisions (by the City Council, the university and others); and
- To identify key place-making principles through a series of themed development principles

## 1.7 Organisation of the SPD

1.7.1 The document is organised into two principal parts. The first seeks to describe and interpret the existing site from a variety of perspectives. The second describes the parameters within which future development should be brought forward. In each case the site is addressed both as an integral part of the wider city and as the major part of a defined urban block. Note that for the purposes of this document, the term “parameter” is only a general reference to a “framework” for change in respect of matters discussed in text and plan (access, entrances and public realm outside the site, proposed open space and the public realm, and built form). The term is not the same as that referred to in the Town and Country Planning (Development Management Procedure) (England) Order 2010, which applies to large scale proposals qualifying as “EIA” development.

## 1.8 Consultation

1.8.1 The SPD ~~will be the~~ was subject of public consultation for a period of six weeks from the 13 July 2015 to the 7 September 2015. ~~The consultation for this SPD will run from:~~

~~9am on 13th July 2015 to 5pm on 7th September 2015~~

1.8.2 ~~There are a variety of ways to respond to the consultation:~~

- ~~Online by visiting:~~ <http://cambridge.jdi-consult.net/ldf/>
- ~~By filling in and returning a response form available from:~~ <https://www.cambridge.gov.uk>

<sup>2</sup> Through PM/5/004 – PM/5/007 within the *Addendum to the Cambridge Local Plan 2014 Proposed Submission Document (July 2013): Schedule of Proposed Changes Following Proposed Submission Consultation (February 2014)*

- ~~By emailing a response form to : [polycysurveys@cambridge.gov.uk](mailto:polycysurveys@cambridge.gov.uk)~~
- ~~By posting a response form to: New Museums Site SPD Consultation, Planning Policy, Cambridge City Council, PO Box 700, Cambridge, CB1 0JH~~

~~1.8.3 The final version of this document will be amended prior to adoption to reflect some or all of the following:~~

- ~~Comments received on this draft document during consultation;~~
- ~~Any amendments to relevant policies in the adopted local plan;~~
- ~~Any governmental policy changes.~~

~~1.8.4 This document will be subject to final consideration and approval by the council's Environment Scrutiny Committee before its adoption.~~

~~1.8.5 A Sustainability Appraisal and Habitats Regulations Screening Report has been carried out and consulted upon for the Cambridge Local Plan 2014: Proposed Submission. This consultation took place between 19 July 2013 and 30 September 2013. These documents will be available to view during this consultation. As the draft SPD supports the Cambridge Local Plan 2014: Proposed Submission, there is no further need to undertake a separate Sustainability Appraisal or Habitats Regulations Assessment for the SPD itself, although screening reports have been completed and will be made available during the consultation.~~

~~1.8.6 An Equalities Impact Assessment will also be made available during consultation, which includes updated information relevant to this SPD.~~

## **1.9 Status of this document**

~~1.9.1 This is the draft version of the New Museums Site SPD, agreed for consultation at Development Plan Scrutiny Sub-Committee by the Executive Councillor for Planning Policy and Transport on 16 June 2015.~~

~~1.9.12~~ Once adopted, the SPD will be a material consideration in the determination of relevant planning applications.

~~1.9.23~~ The draft SPD has been written to support the Cambridge Local Plan 2014: Proposed Submission. This SPD will be adopted at the same time as, or shortly after, the local plan is adopted. It will not be adopted before the local plan is adopted.

## **2 PLANNING CONTEXT**

### **2.1 Cambridge Local Plan (2006) and the Cambridge Local Plan 2014: Proposed Submission**

2.1.1 The NMS was first allocated in the Cambridge Local Plan 2006 as Site 7.08. It was allocated for “redevelopment/refurbishment for predominantly University uses, with some mixed use to enhance the attractiveness of the public realm.” This allocation required the development of a planning brief. At the time of preparing this SPD, Policy 43 University Faculty Development and Appendix B: Proposals Schedule within the Cambridge Local Plan 2014: Proposed Submission identify the NMS as Site U2 for mixed-use redevelopment for university related uses. The Cambridge Local Plan 2014: Proposed Submission document is currently being examined by an independent Inspector.

2.1.2 Upon adoption, the SPD will represent a site development brief and provide greater certainty and detail to support the delivery of this allocation in the coming years.

### **2.2 Masterplan**

2.2.1 A well prepared, clear and informed Masterplan for proposed and future development of the New Museums Site is vital in ensuring coordinated development. The University has prepared a site Masterplan which illustrates how the proposals set out in this SPD could be implemented, as set out in Plan 2 below.

2.2.2 As part of the preparation of the Masterplan, there was significant engagement between the University, City Council and Historic England, which helped add detail to the proposals set out in this document and provided a brief for the development of different areas on the site.

2.2.3 The Masterplan will not be formally adopted with the SPD, but the Masterplan proposals will be submitted with applications for each phase of development. The Masterplan has helped inform the parameters set out in this SPD and the illustrative master plan contained herein represents a more detailed illustration of these parameters. The Council will require each proposal for development to be compliant with this SPD.

2.2.4 The Masterplan will be updated by the University when necessary. This may include updates after completion of each phase of development.

# THE NEW MUSEUMS SITE, DEVELOPMENT FRAMEWORK SPD

## THE NEW MUSEUMS SITE PLAN NO 2: ILLUSTRATIVE MASTERPLAN



### LEGEND

①	Downing Street	⑤	Bene't Street	Ⓐ	First Courtyard
②	Corn Exchange Street	⑥	Grand Arcade	Ⓑ	Second Courtyard
③	Peas Hill	⑦	Downing Site	Ⓒ	Third Courtyard
④	Free School Lane				

## **2.3 University Estate Strategy**

2.3.1 The mission of the University of Cambridge is to contribute to society through the pursuit of education, learning and research at the highest international levels of excellence. Its Estate Strategy (2012) supports this mission by setting out the strategy for the development of the estate and the creation of buildings and places that support world class teaching and research and efficient administration.

2.3.2 The main aims of the strategy are:

- To meet priority needs for new and or improved facilities
- To cluster associated University disciplines, in order to achieve academic benefits and administrative operating efficiencies
- To provide buildings and places with high levels of sustainability and design quality
- To deliver optimum space efficiency

2.3.3 The University Estate Strategy supports the development of the New Museums Site as the centre for social sciences, conservation biology and student support services within the University. The site is therefore the focus of a sequence of proposed institutional moves including the following.

To West Cambridge from the NMS:

- Dept. of Materials Science and Metallurgy;
- Dept. of Chemical Engineering and Biotechnology
- University Information Services including its Data Centre

To the NMS from other sites:

- Dept. of Geography (from the Downing Site)
- Dept. of Land Economy (from the Old Press Site)
- Student Services ( from various city centre sites)

2.3.4 The University Estate Strategy is supported by a Capital Plan and a Capital Projects Process which focus resources into strategically important projects.



### **3. VISION AND OBJECTIVES**

#### **3.1 Vision**

##### **3.1.1 The Vision for the future development of the site is:**

The New Museums Site will develop as a place for University of Cambridge related uses involving world class teaching, learning and research. The quality of place will improve through the construction of new university facilities, a reduction in development density in the site core, an associated improvement to environmental conditions, and the provision of high quality public realm within the site. Development will support the creation of an attractive, accessible, safe and sustainable environment in line with the following objectives.

#### **3.2 Objectives**

##### **3.2.1 The Objectives for the development of the site are:**

##### **Objective 1: To capture the history and tradition of the Site**

The NMS has an impressive academic heritage that includes ground-breaking research and discoveries. Development of the site will promote the understanding of the site's heritage whilst transforming it into an attractive and sustainable environment for working and learning, while making it adaptable for future requirements. The site will continue to contribute to the life of the city, as a focus for the University museums and collections and will create a 'window' into the life of the University and the site's history through enhancement of public access and the creation of enjoyable spaces.

##### **Objective 2: To improve accessibility**

Entrances to the site from surrounding city streets will be improved, inviting access and controlled public use of the site. This will benefit staff, students and visitors, and enable the site's museums and collections to be more easily accessed, and its cultural heritage to be explored.

##### **Objective 3: To retain heritage and quality buildings that contribute to the site and its surroundings**

Development of the site will selectively conserve heritage buildings, located principally in the perimeter of the site, comprising a majority of Listed Buildings and Buildings of Local Interest. The core of the site has been substantially changed over time, and contains buildings of generally lower significance. The site core therefore provides the greatest opportunities for change and for new open spaces, which will in turn improve the setting of Listed Buildings on the site's perimeter. Some core buildings are of significance and those of greatest significance will be retained.

**Objective 4: To provide a new spatial structure with links to the city**

A clear spatial structure for the site will be created, as a part of the historic city that supports University activity and provides a platform for academic life. This will include an attractive and legible environment that allows for all uses to come together with attractive open spaces that have a high amenity and sustainability value, as well as supporting the individual building uses. New flexible open spaces will be provided that allow for the movement of people and a setting for buildings.

**Objective 5: To embrace sustainability**

The City Council supports the preparation and implementation of a bespoke sustainability framework for the New Museums Site. This will relate to sets of sustainability principles grouped around the themes set out in Table 1 below:

Table 1.

Sustainability Theme	Sustainability Principles
Resource and Climate Change	Energy and Climate Change Water Materials Waste
People's Health, Social and Economic Wellbeing	Health and Wellbeing Collaboration and Inclusion Education and Knowledge Transfer Employment Opportunities
Land Use, Ecology and Local Impact	Biodiversity and Ecology Pollution and Local Environment Culture, Heritage and the City
Transport and Local Connectivity	Transport and Mobility

There are significant opportunities to create ecological habitats on the site on green/brown roofs, and to reduce the carbon footprint of the site through the provision of low carbon energy systems and renewable energy production.

#### **4. THE EXISTING SITE**

##### **4.1 Location**

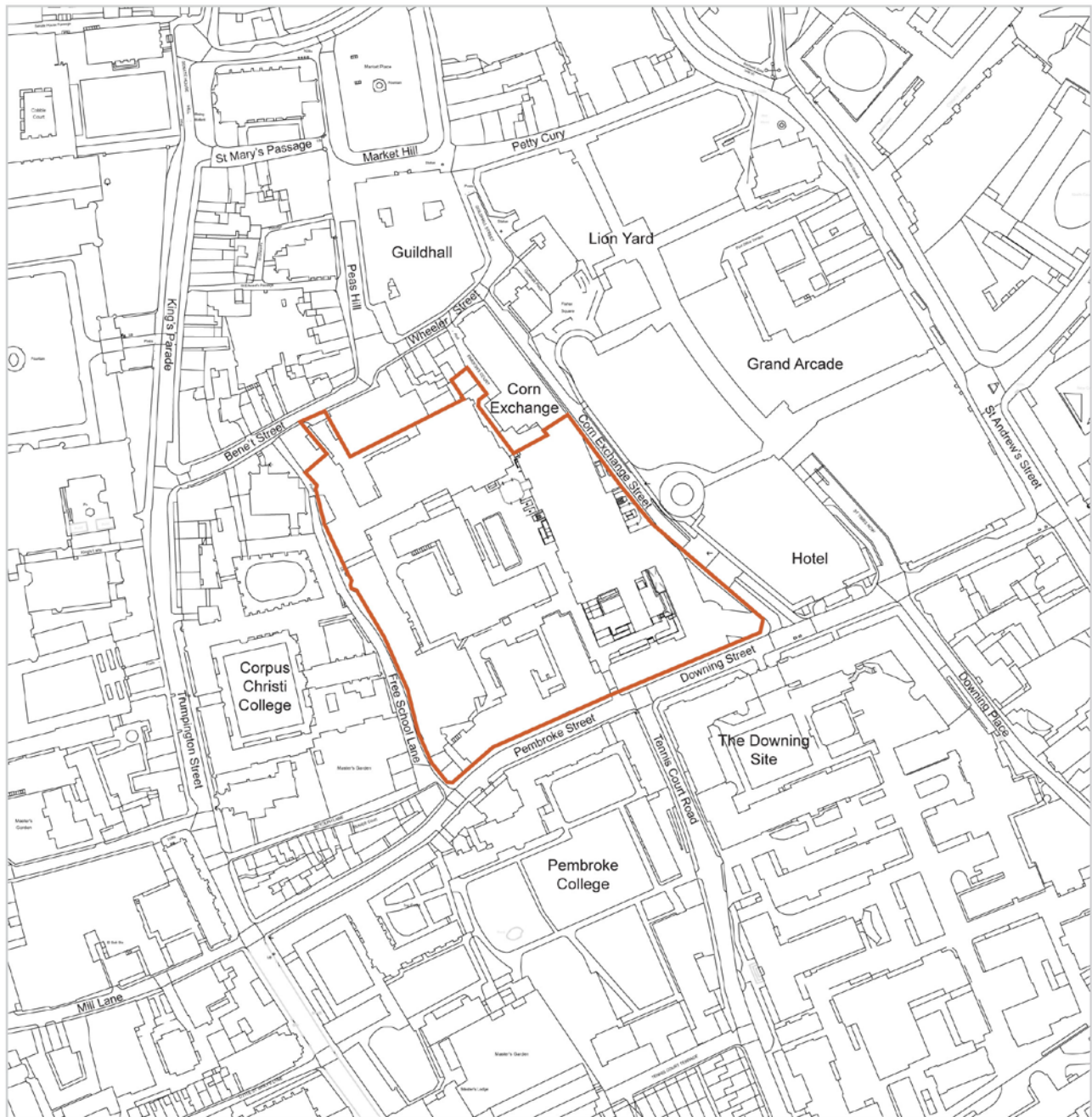
4.1.1 The NMS, which is one of the University's central and most prominent sites, occupies an historic location in the city centre with a unique and globally important heritage. It consists of the greater part of the urban block bounded by Pembroke Street, Downing Street, Corn Exchange Street, Wheeler Street, Bene't Street and Free School Lane. The buildings along the northern edge of the block fronting onto Wheeler Street and Bene't Street, including the Corn Exchange, are outside the site.

4.1.2 As illustrated in Plan 3 below, the site sits between the area of College development along Trumpington Street and the commercial core of the town along St. Andrews Street. Pembroke College and the University's Downing and Old Addenbrooke's sites are to the south; Corpus Christi College is to the west; the Corn Exchange and Grand Arcade shopping areas are to the east and the Guildhall and Market Square are to the north. As such the site has an important role in mediating between the scale of modern commercial development to the east and that of older college buildings to the west, and between the busy vehicular highway that is Pembroke/ Downing Street to the south and the more pedestrian orientated area around the Market Square to the north.

# THE NEW MUSEUMS SITE, DEVELOPMENT FRAMEWORK SPD

## THE EXISTING SITE

### PLAN NO 3: SITE LOCATION PLAN



#### LEGEND



## 4.2 Historical Development of the Area and its Significance

### 4.2.1

What is now the New Museums Site is at the heart of the medieval city of Cambridge. Its early known history is as the site of an Augustinian Friary. The Friary was founded in 1290 but following the dissolution of the monasteries (1536-39) it was surrendered to the Crown. Late 16th century maps (Lyne 1574, Smith 1588 and Hammond 1592) show that significant elements of the Augustinian Friary were still evident at this time. The King's Ditch (thought to be a Saxon defence) runs across the south-eastern corner of the site. To the north of the site, the market place was the economic and social core of the town.

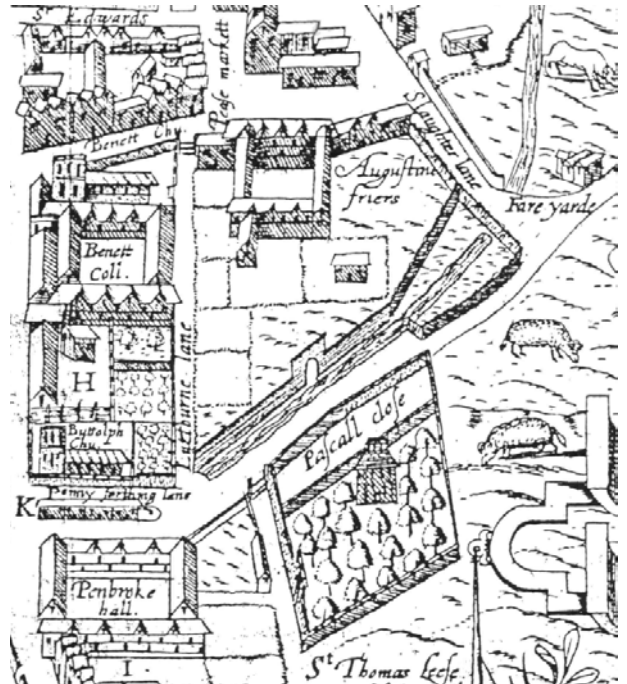


Figure 1 Lyne 1574

### 4.2.2

From the 17th century onwards, overcrowding in Cambridge meant that available land within the city was increasingly valuable and was gradually developed. The former Augustinian Friary land, although still significantly open, was encroached upon along its street frontages and in the 1620s, The Free School was constructed in the southwest corner of today's New Museums Site. The remaining elements of the former Friary buildings were subsumed into the range of courts and buildings fronting what is now Wheeler/Bene't Street. By the end of the 17th century, despite the increasing development on the site, much of the land was still open gardens, including the triangle of land south of the King's Ditch.



Figure 2 Loggan 1688

4.2.3 The 18th century marked the beginnings of an era of more substantial changes to the New Museums Site, with the development of a large mansion house (Mortlock's house and then bank) and associated outbuildings at the north end of the site. In 1760, the majority of the site was bought by Richard Walker, Vice-Master of Trinity College for the University to provide a Botanic Garden. This was established in 1762 and was accessed from a pair of gates on Pembroke Street and a small archway along Free School Lane. By the end of the 18th century, apart from the Botanic Garden, there was no longer any undeveloped land in any quantity within the central core of Cambridge except for The Leys (marshy land) on the south side of Pembroke Street.

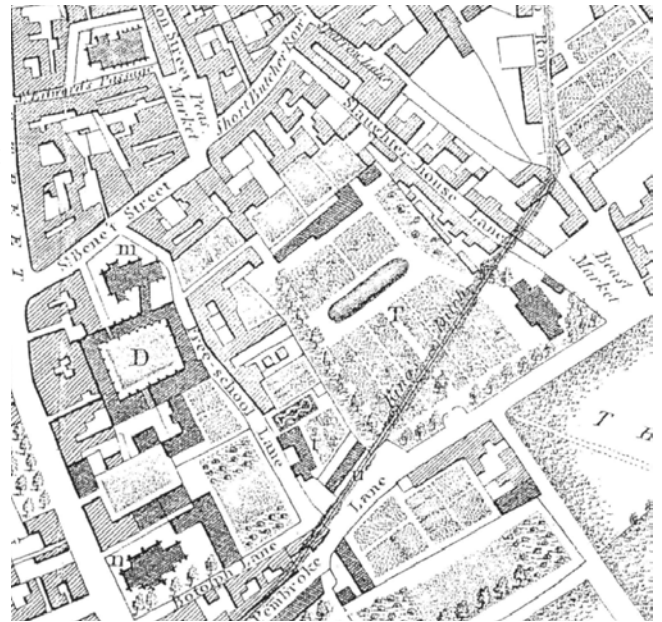


Figure 3 Custance 1798

4.2.4 The enclosure of Cambridge in the early years of the 19th century, changed the wider topography of the town, with rapid development occurring predominantly to the east initially and then to the south. Within the New Museums Site, however, little changed until 1832 when buildings by Charles Humfrey were constructed for the use of the Professor of Anatomy on the corner of Downing Street and St Andrew's Hill. The Leys, on the opposite side of Pembroke Street became the site of Downing College and construction began in 1821. 19th century maps no longer show the King's Ditch running through the site, with the line of the ditch only dotted on Baker's Map of 1830.



Figure 4 Baker 1830



- 4.2.5 More major change came with the establishment of the Natural Science Tripos in the mid-19th century which highlighted the need for better accommodation for this rapidly expanding area of the University. This coincided with the realisation that the Botanic Garden was not fit for purpose and it was removed to its present site off Hills Road between 1846 and 1852 when the site was sold to the University for building 'new museums and Lecture Rooms' (Willis & Clark). A detailed report setting out the requirements was prepared in



Figure 5 Old Cavendish

1853 and in 1854 when the architect Anthony Salvin was authorised to prepare plans. However, the costs of the plans were not well received and, despite numerous changes over the years, indecision over the development of the site continued until 1863 when Salvin was finally given approval to begin construction of the central buildings on the site. These were followed by the Cavendish Laboratory (completed by 1873) and further buildings to the rear of Salvin's Museum of Botany and along Corn Exchange Street.

- 4.2.6 By 1883, the site was largely in the ownership of the University, with only the northern frontage facing the present day Bene't Street still privately owned. The Perse School became part of the site by 1890 and was remodelled to form engineering laboratories, while the Perse Almshouses were replaced in 1886-8 by chemical laboratories, lecture rooms and a porter's lodge. Despite this development, however, a good deal of the former Botanic Garden remained undeveloped.
- 4.2.7 The early years of the 20th century saw the enclosure of much of the Site's boundaries with buildings including the Zoology Building which replaced earlier buildings along Pembroke Street and Corn Exchange Street. A reasonable portion of the old Botanic Garden and its entrance gates on Pembroke Street still survived however. The Examination Halls were completed in 1909 and the adjoining Arts School, designed as lecture rooms and a departmental library, was completed in 1911.
- 4.2.8 Scientific advances in the 20th century required new and enhanced teaching spaces and buildings were quickly erected or altered on the New Museums Site in the first half of the 20th century. Considerable consolidation of the site occurred with development on an ad-hoc basis as necessity arose and/or funding became available.

4.2.9 The site was described as ‘*an incredible muddle inside*’ by Nikolaus Pevsner writing in 1970. By this time, the need for comprehensive redevelopment of the site had already been established and in 1961, Denys Lasdun had published a master plan for the site, but this was not well-received by the City and County Councils largely due to the excessive height of some of the buildings. A long planning wrangle ensued before a much reduced scheme was approved in 1964 and the job passed to the newly formed Arup Associates. Only Stage 1 of the redevelopment was ever completed. This was to become known as the Arup Building and has recently been renamed the David Attenborough Building.

4.2.10 The erection of the Arup Building involved the demolition of almost all of the remaining Salvin buildings in the centre of the NMS. Such wholesale redevelopment was being replicated on the other side of Corn Exchange Street with the construction of the Lion Yard shopping centre and car park to which the Arup Building was originally intended to link. The historical features of the site are illustrated in Plan 4 below.



Figure 6 Lowry 1863

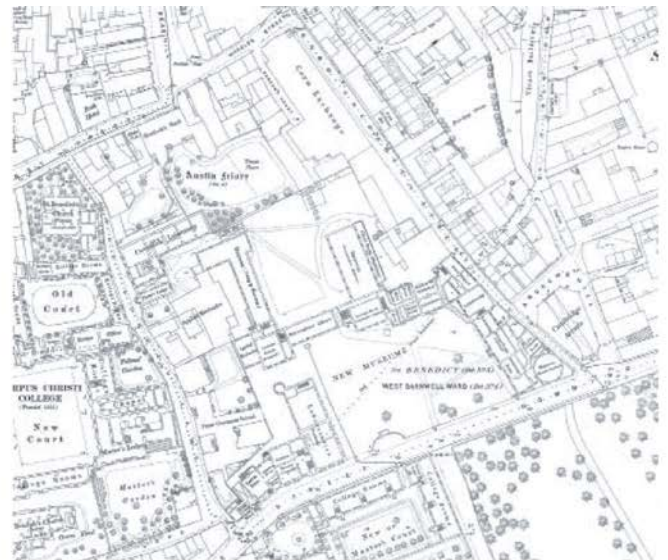


Figure 7 1888 OS Map

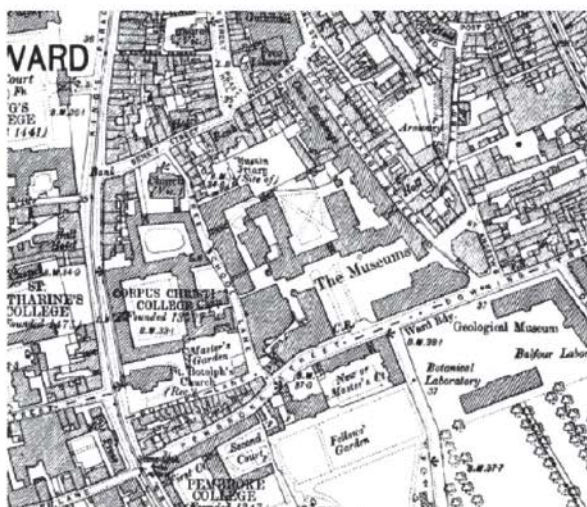


Figure 8 1903 OS Map

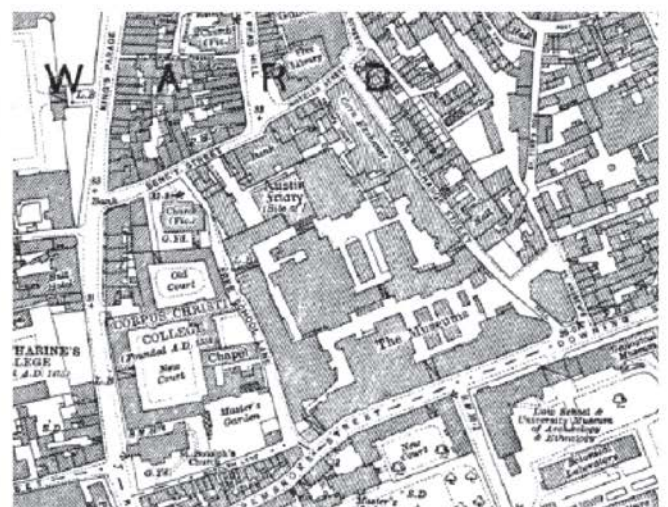
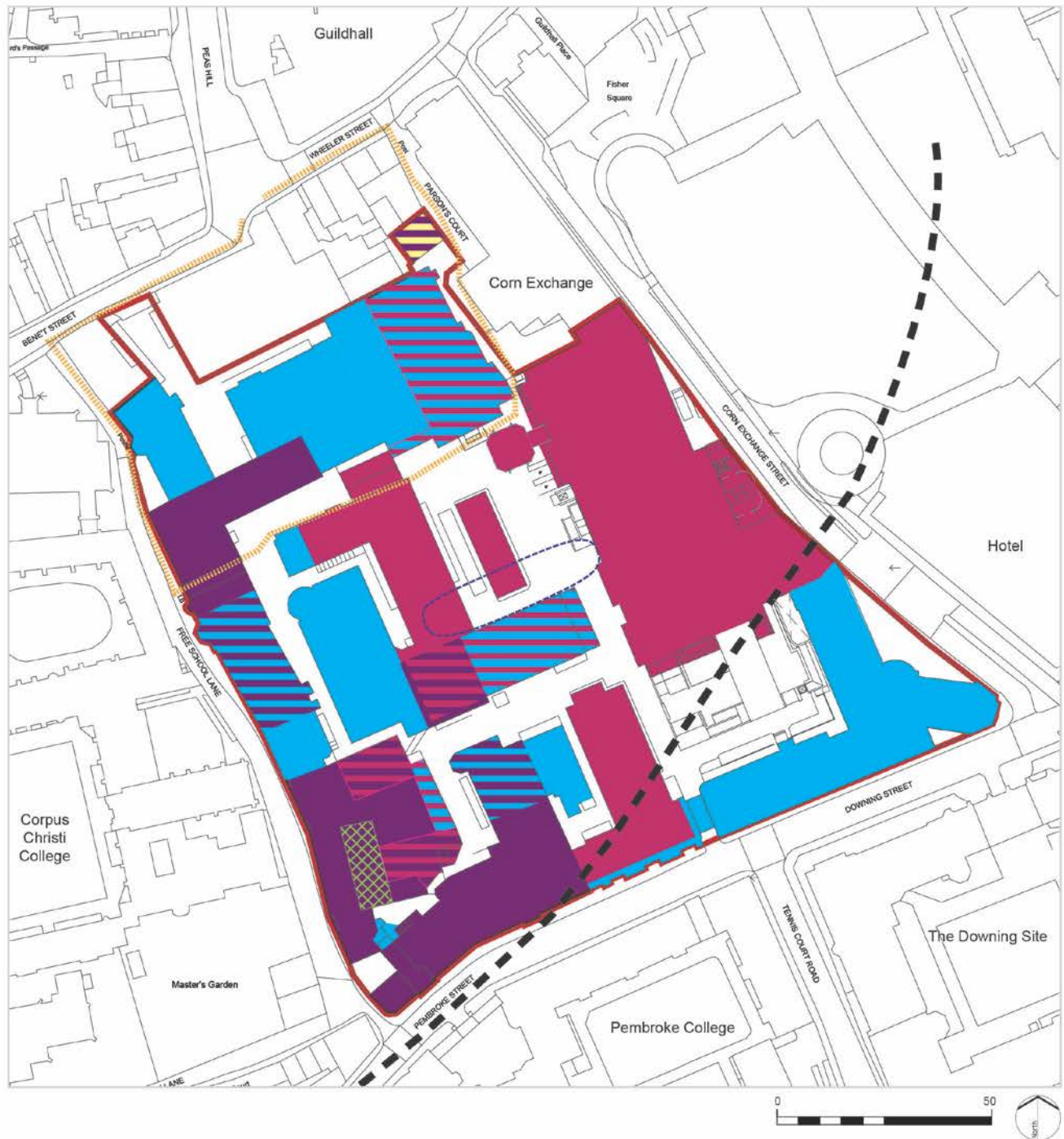


Figure 9 1927 OS Map



# THE NEW MUSEUMS SITE, DEVELOPMENT FRAMEWORK SPD

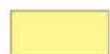
## THE EXISTING SITE PLAN NO. 4 HISTORICAL FEATURES



### KEY



C17



C18



C19



Early C20 (Pre WWII)



Late C20 (WWII & later)



New Museums Site



Approx. line of King's Ditch



Probable area of main Augustinian Friary buildings



Approx. position of Pond in Botanic Garden, possibly Monastic in origin

### 4.3 Existing Heritage Assets

#### a. Archaeology

- 4.3.1 The site lies in an area of varying archaeological potential. There is generally considered to be low potential for prehistory remains and low to moderate potential for Roman remains. The potential for medieval remains is however high as the site's boundaries encompass the precinct of the Augustinian Friary and the course of the King's Ditch runs across the southeast corner of the site.
- 4.3.2 Plan 5 shows the predictive model of the areas in which archaeology is most likely to survive on the NMS. This deposit model has been built using extrapolated data from nearby intensively investigated sites (e.g. the Grand Arcade) together with what little formal archaeological information on the site (e.g. the excavation work undertaken before World War I) exists. This has been combined with surface, ground and basement level heights to produce the predictive model which indicates the areas where there is potential of greatest impact upon any underlying archaeological remains.
- 4.3.3 Archaeological remains have previously been identified within the site during the excavations for the foundations of the Arts School and Examinations Rooms in 1908 at the northern end of the site. These investigations identified Friary structures and a cemetery. In 1991, work inside the Old Cavendish Laboratory building found substantial masonry remains belonging to the friary (Cambridge Archaeological Unit Report 037, CAB 91). The site also straddles the line of the King's Ditch which was identified in the neighbouring (to the west) Grand Arcade development and recorded as being 10-12 metres wide and 3.5 metres deep (Cambridge Archaeological Unit Report Number 800, ECB2379 and ECB2389).
- 4.3.4 The location of the site, within the historic core of Cambridge, encompassing the precinct of the Augustinian Friary (CHER Number 04731) and spanning the city's defences, is highly significant and any surviving archaeological remains in this area will be key to our understanding of the early development of the town. The development of this area offers the potential to considerably advance the understanding of the layout of the friary as the precise location and form of the friary remains unclear.
- 4.3.5 Any investigation of and, ultimately mitigation for, the archaeology will adhere to the principles outlined in national, local and industry guidelines which favour the preservation in situ of significant archaeological remains where they have been identified. Where preservation is not ~~practicable~~ required, an appropriate level of recording of the archaeology will be completed prior to further work.

# THE NEW MUSEUMS SITE, DEVELOPMENT FRAMEWORK SPD

THE EXISTING SITE  
PLAN NO.5 ARCHAEOLOGICAL POTENTIAL



## KEY

- Projected Line of the Kings Ditch
- × Kings Ditch Buffer Zone

## Potential Depth of Deposits

Value  
High : 3.2  
Low : -3.2

Potential for Surviving Deposits Relative to the  
Projected Height of Underlying Natural Geology

0 5 10 20 30 40 Meters



This diagram relies on extrapolated data for the height of underlying natural. It provides a general picture of the potential for archaeological deposits, but it cannot be used to indicate the likelihood of survival at any single point.

b. Listed Buildings

4.3.6 There are a number of Listed Buildings within and directly adjoining the site. These buildings are indicated on Plan 6. There is a statutory duty to have special regard to the desirability of preserving any Listed Building or its setting, or any features of special architectural or historic interest which it possesses (Sections 16 and 66 of the 1990 Act).

4.3.7 In summary, the Listed Buildings (all Grade II) within the site that need to be considered as part of any proposals are:

- Mond Building
- Zoology Building
- Old Physical Chemistry
- Cavendish Laboratory
- Arts School

c. Conservation Area

4.3.8 The entire site lies within the Cambridge Historic Core Conservation Area which is itself part of the large Cambridge Conservation Area No.1. There is a statutory duty to preserve or enhance the character and appearance of the conservation area (conferred by Section 72 of the Planning [Listed Buildings and Conservation Areas] Act 1990).

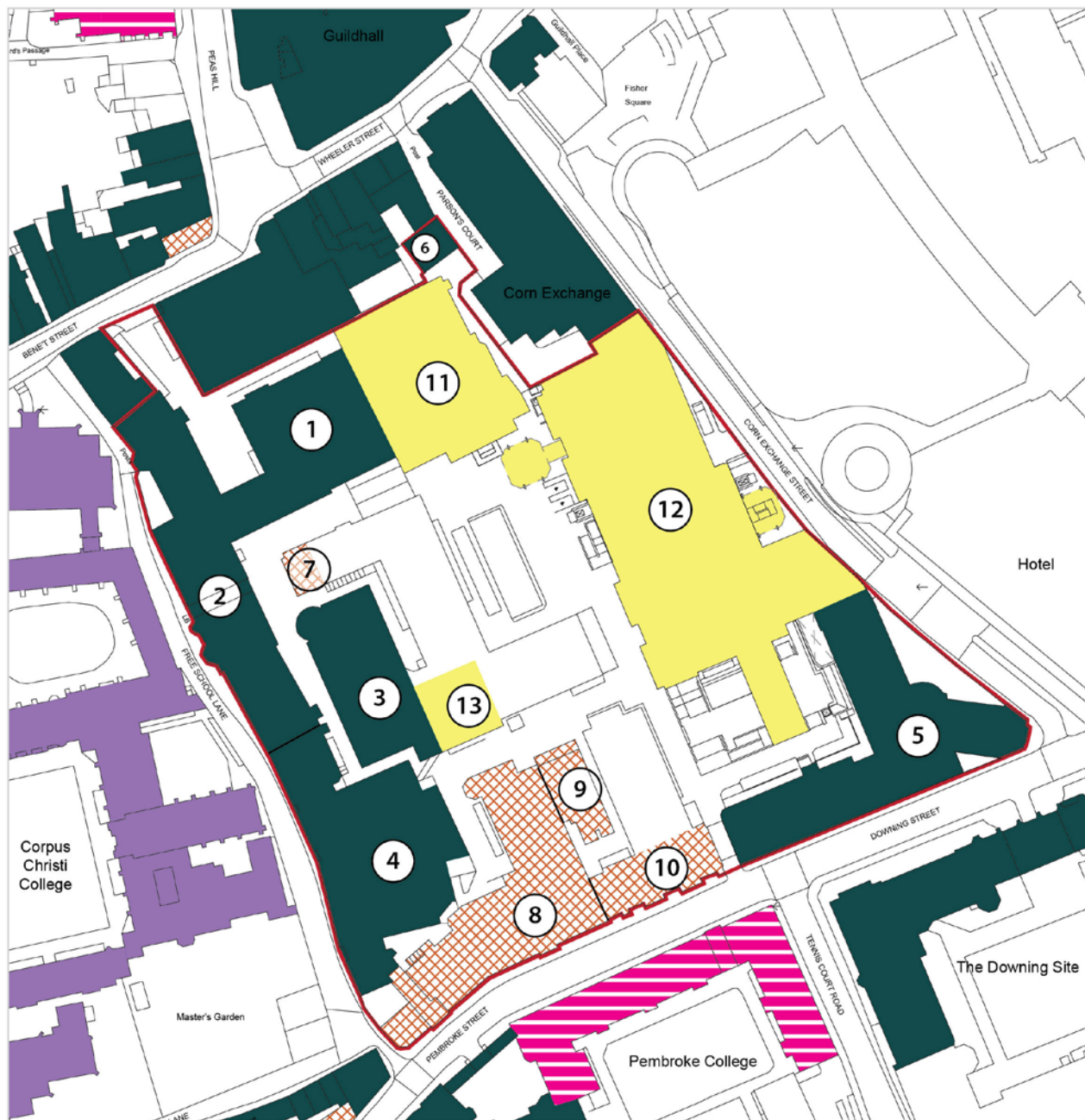
4.3.9 The Conservation Area is a designated heritage asset, along with the Listed Buildings, and the contribution that the site makes to the character and appearance of this heritage asset must therefore be considered. Detailed assessment of the streets that the New Museums Site forms part of is provided in the Cambridge Historic Core Conservation Area Appraisal that forms part of the background evidence to this SPD.

<https://www.cambridge.gov.uk/historic-core-appraisal>



# THE NEW MUSEUMS SITE, DEVELOPMENT FRAMEWORK SPD

## THE EXISTING SITE PLAN NO. 6 HERITAGE ASSETS



### KEY

Grade I Listed

Grade II\* Listed

Grade II Listed

Building of Local Interest

Other buildings of heritage interest

New Museums Site

1 Arts School

2 Cavendish Laboratory

3 Mond Building

4 Old Physical Chemistry

5 Zoology Building

6 4 Parson's Court

7 Mond Building Annexe

8 Old Chemical Laboratories

9 Goldsmith's Laboratory

10 1907 Building

11 Examinations Halls

12 David Attenborough Building

13 Balfour Building



d. Buildings of Local Interest

4.3.10 The site and surroundings also contain a number of Buildings of Local Interest (BLIs) which are 'non-designated heritage assets' as defined in the National Planning Policy Framework. Works to such buildings are controlled by a specific Local Plan policy (Policy 62: Local heritage assets) in addition to national planning policy. These buildings are indicated on Plan 6.

4.3.11 The non-designated heritage assets (Buildings of Local Interest, otherwise known as "BLI's") within the site that need to be considered as part of any proposals are:

- Mond Building Annexe
- Old Chemical Laboratories and Porter's Lodge
- Chemical Laboratories extension (1907 building, the frontage of the Shell Building)
- Goldsmith's Laboratory

e. Other buildings of heritage interest

4.3.12 In addition to the designated and non-designated heritage assets, some other buildings on the site have been identified as having some heritage interest. They have no individual heritage designation, but are buildings within the conservation area:

- David Attenborough (Arup) Building
- Balfour Building
- Examination Halls

f. Significance

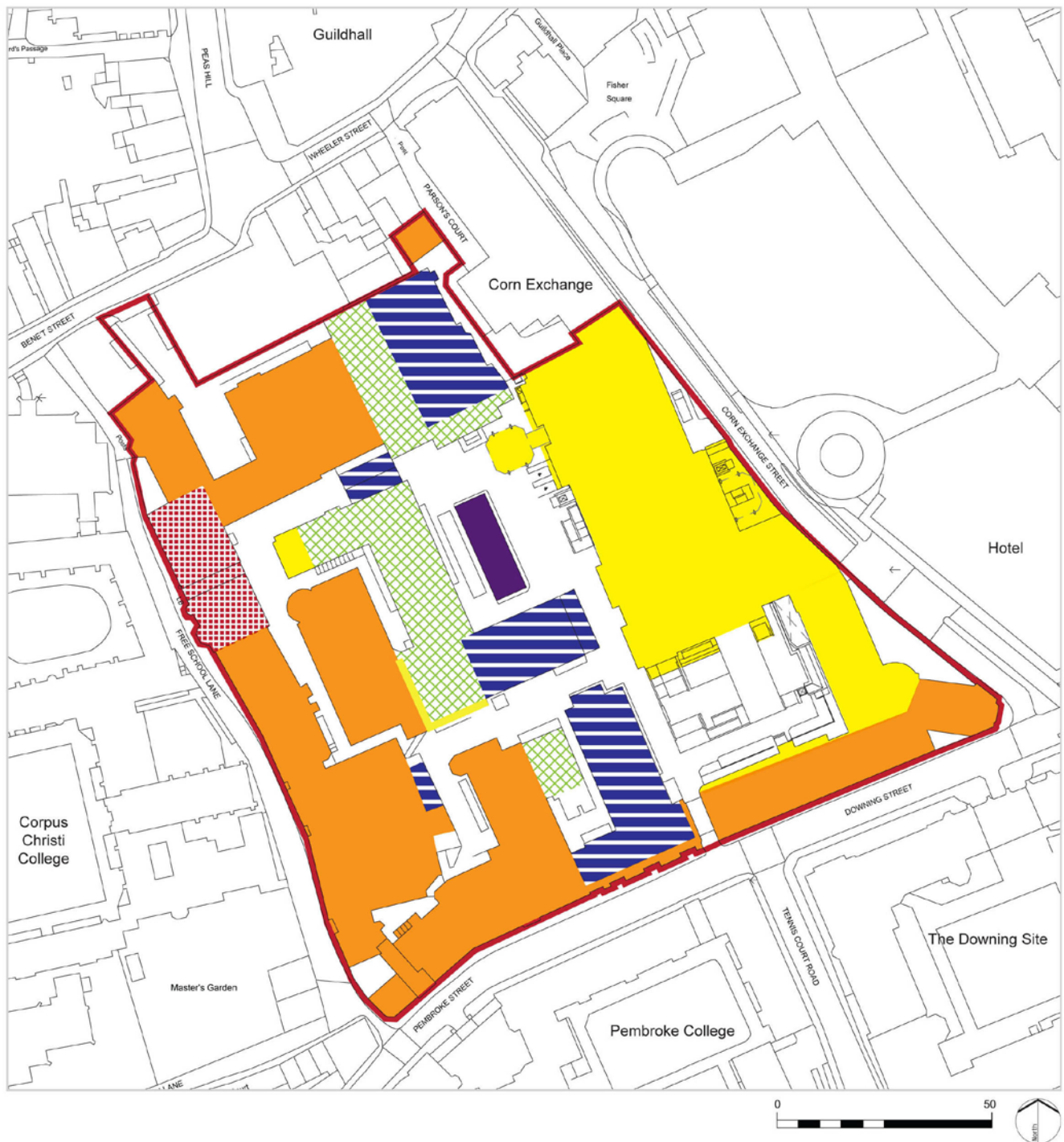
- 4.3.13 Building functions and uses within the wider area have changed considerably over the centuries. The site's significance is now based on its function for University teaching (primarily originally sciences) and as a key block in the historic core of the city. The cultural significance of the site in terms of its scientific history and its previous uses as the University's original Botanic Garden, the site of the original Perse School and as an Augustinian Friary is however lesser-known. Allied to this, is the archaeological significance of the site which may contain surviving remains of earlier phases of development including the locally important King's Ditch and Augustinian Friary.
- 4.3.14 The site contains a variety of building types, although they are all unified by their University use. They vary between adaptations of existing buildings, such as No.4 Parsons Court and Old Physical Chemistry, and purpose-built University buildings, although these too have all been subject to substantial remodelling to suit changing University needs.
- 4.3.15 A detailed assessment<sup>3</sup> of the site including each building has been completed. The level of architectural, historical and townscape significance attributed to each building is illustrated on Plan 7. Significance will normally be measured as follows:
- *Very High*: Usually reserved for designated heritage assets of the highest importance, i.e. Grade I and II\* Listed Buildings and Historic Parks and Gardens and Scheduled Monuments.
  - *High*: Grade II Listed Buildings
  - *Significant*: Non-designated heritage assets (e.g. BLIs) or Buildings of considerable townscape interest and/or intrinsic architectural or historic interest
  - *Moderate*: Buildings of some townscape interest and/or architectural or historic interest, but which have no formal heritage status
  - *Low*: Buildings which are neutral in townscape terms and/or have little architectural or historic interest
  - *None*: Buildings of no or insignificant townscape or heritage interest.
- 4.3.16 N.B. A building may move up a significance category if it is of considerable townscape importance and integral to the character and appearance of the conservation area. Similarly, a building may move down a category if it has been assessed, following investigation/research, to have lesser heritage interest than its status may imply.

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<sup>3</sup> The New Museums Site Historic Environment Analysis (Beacon Planning Ltd. 2015)

# THE NEW MUSEUMS SITE, DEVELOPMENT FRAMEWORK SPD

## THE EXISTING SITE PLAN NO.7 HERITAGE SIGNIFICANCE



### KEY

	Very High		Moderate		New Museums Site
	High		Low		
	Significant		None		

#### **4.4 Land Ownership and Use**

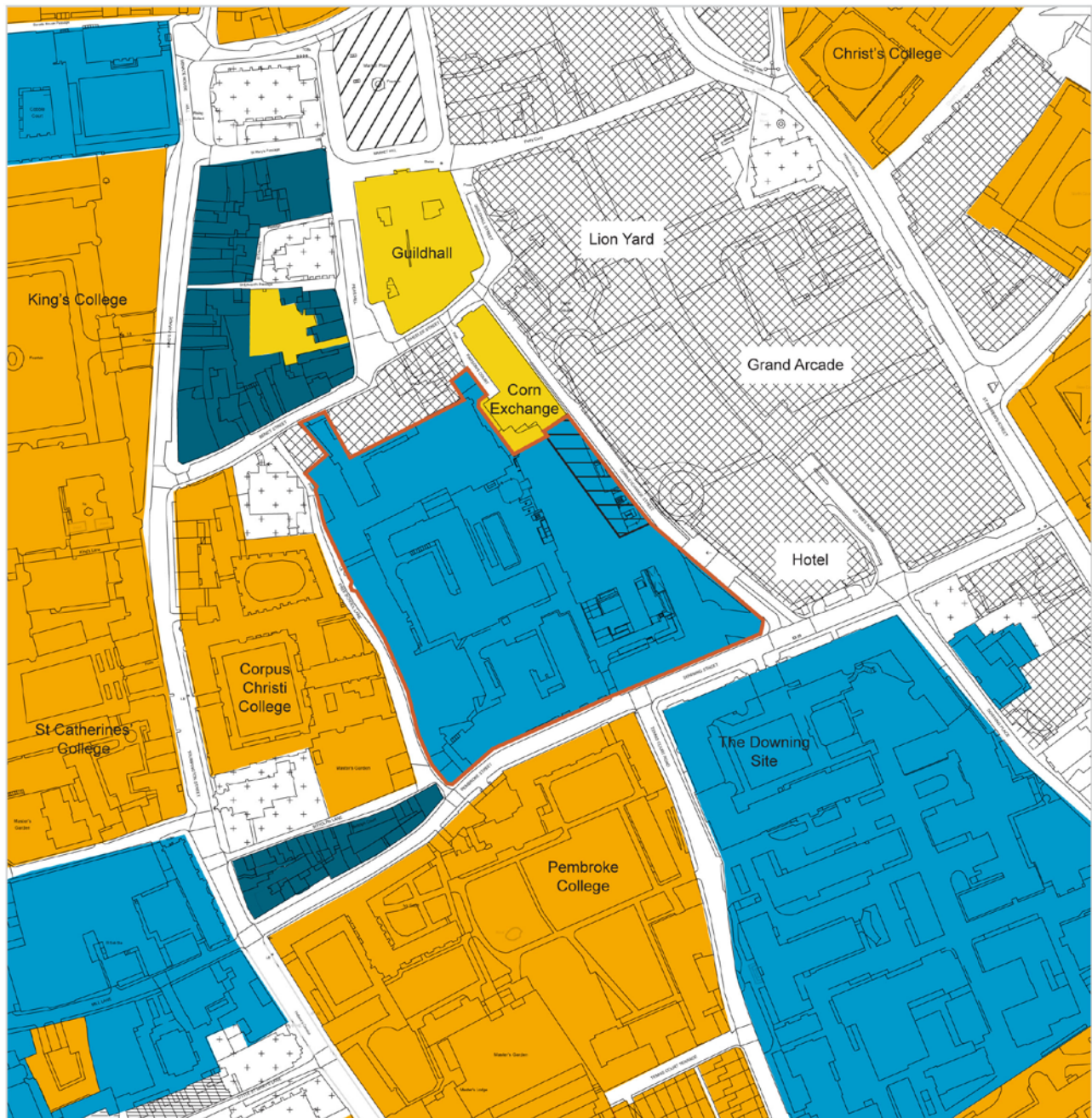
- 4.4.1 The whole of the site is either owned by the University of Cambridge or held by it on long leases, as shown on Plan 8. There are approximately fifteen different University institutions on the site including the Dept. of Zoology, the Dept. of the History and Philosophy of Science, the Dept. of Sociology and the Divisions of Biological and Social Anthropology within the Dept. of Archaeology and Anthropology. There are also a variety of support functions such as central library facilities and a number of large lecture theatres including the 450 seat Babbage Lecture Theatre; one of the largest in the University.
- 4.4.2 All of this is used almost exclusively for teaching and research. Two of the University's museums, the Museum of Zoology and the Whipple Museum of Science, which are used for teaching and research but also encourage non specialist visitors, are located on the site. The University's Sedgwick Museum of Earth Sciences and the Museum of Archaeology and Anthropology are located close by on the south side of Downing Street. In addition, the University will be providing accommodation for the Cambridge Conservation Initiative in which a number of non-university conservation organisations will be tenants from 2016.
- 4.4.3 The site is surrounded by a wide variety of land uses as is typical of Cambridge city centre, with Colleges, shops, restaurants, businesses and civic and cultural amenities all within a short distance.



# THE NEW MUSEUMS SITE, DEVELOPMENT FRAMEWORK SPD

## THE EXISTING SITE

### PLAN NO 8: LAND OWNERSHIP & USE



#### LEGEND

	Site Boundary		College Owned (but including other commercial uses)		Residential
	University Ownership		Civic Building		Market Square
	Held by the University on a long lease		Churches		Commercial & Retail
	College Ownership				



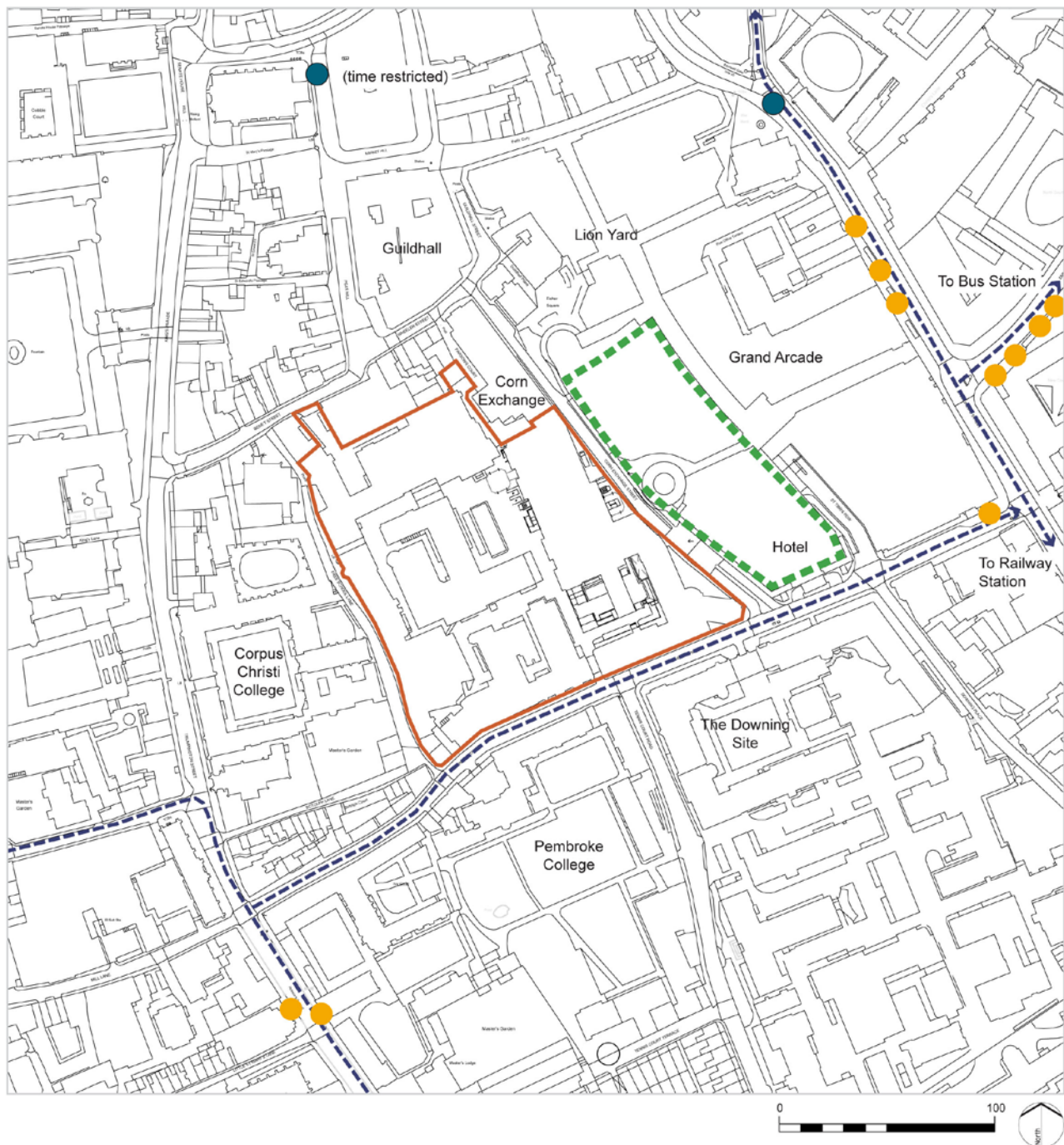
## **4.5 Transport Connections**

- 4.5.1 The site is well connected to the wider city beyond the immediate city centre in terms of public transport, as shown on Plan 9. The Trumpington Park and Ride bus stops on Downing Street itself, the Uni 4 passes the end of Pembroke Street along Trumpington Street to the other main University sites and the main bus station is only a few hundred yards away. A taxi rank is also nearby on St Andrews Street and the railway station is approximately 1 mile away with links to London, Stansted Airport and other major transport hubs.
- 4.5.2 Downing Street and Pembroke Street are a part of the main one way traffic route through the town centre and provide access to Corn Exchange Street from which the city centre car parks are reached. As such they are busy roads and queues frequently form along these roads along the southern edge of the site. A pelican crossing links the site to the University's Downing Site to the south east but the quality of the environment for pedestrians - with narrow pavements of inadequate width to accommodate the numbers of users, especially at busy times, and those with particular needs - could be improved.
- 4.5.3 Large vehicles, including articulated lorries, use the route along Pembroke Street, Downing Street and Corn Exchange Street to service the Corn Exchange and the Arts Theatre as well as commercial premises along Wheeler Street and Benet Street.
- 4.5.4 There are contraflow cycle lanes on all the surrounding streets. Cycle hoops are located on Free School Lane.

# THE NEW MUSEUMS SITE, DEVELOPMENT FRAMEWORK SPD

## THE EXISTING SITE

### PLAN NO 9: PUBLIC TRANSPORT



#### LEGEND

-  Site Boundary
-  Area of public car parks
-  Bus Routes
-  Bus Stop
-  Taxi Rank

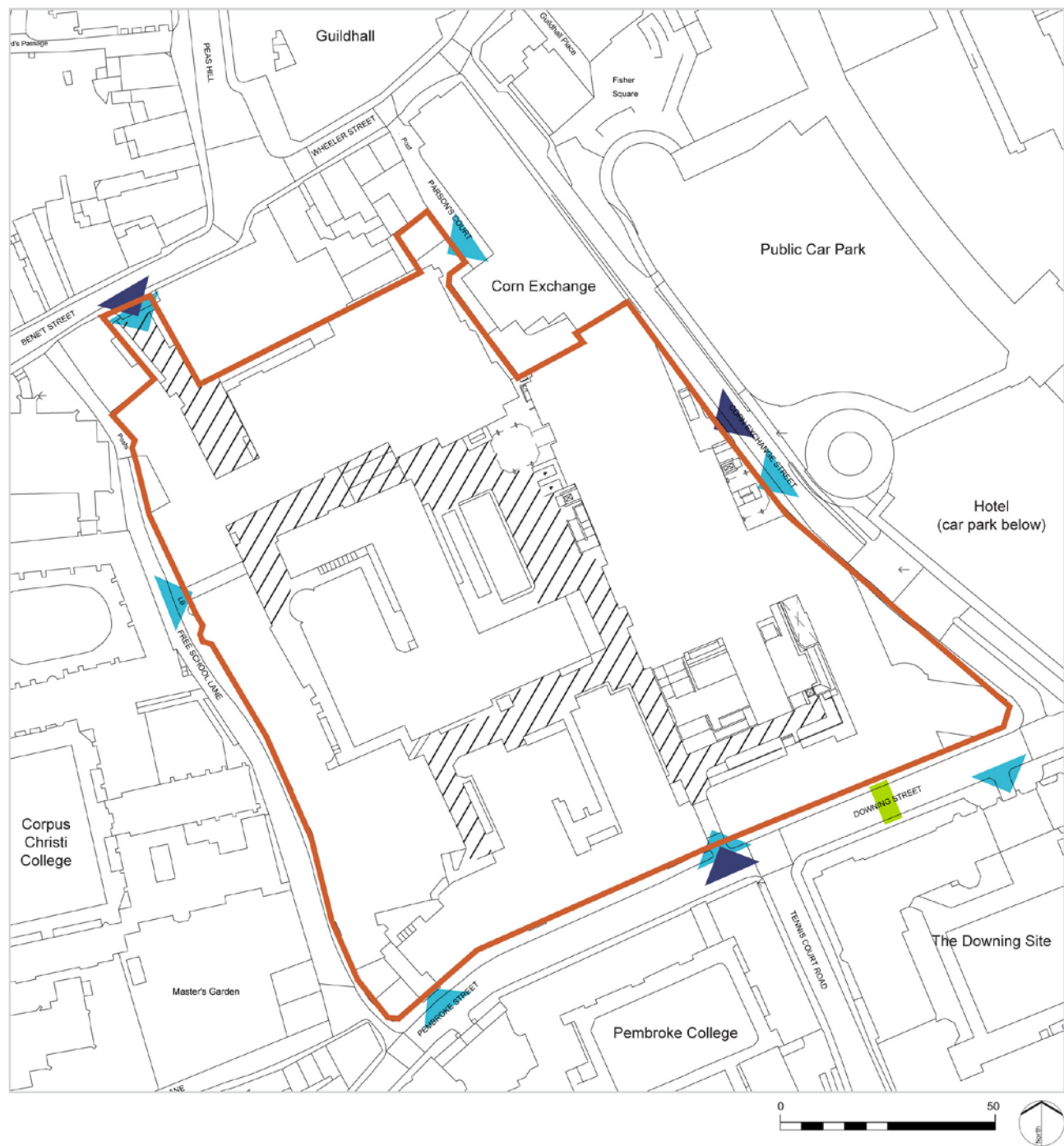
#### **4.6 Access into the Site**

- 4.6.1 Access into the site is currently very poor, as highlighted by Plan 10. The main entrance is through the arch in the Zoology Building on Pembroke Street, almost opposite Tennis Court Road. This provides the only vehicular access to the interior of the urban block and there are significant problems caused by the needs of different users. This is a particular problem in term time when there are large numbers of undergraduate students accessing the large lecture theatres on the site.
- 4.6.2 Other places for pedestrian access to the site for the public are via the entrances in the Old Cavendish Laboratory at the northern end of Free School Lane, the Heycock Wing at the south west corner of the site on Pembroke Street, and via an alley adjacent to the Corn Exchange.
- 4.6.3 A service yard with car parking spaces is accessed from the north via Bene't Street. Members of the University can access the NMS through the Arts School building, the entrance to which is via this yard.
- 4.6.4 There is a layby for the delivery of goods on Corn Exchange on the east side of the David Attenborough Building.






# THE NEW MUSEUMS SITE, DEVELOPMENT FRAMEWORK SPD

## THE EXISTING SITE

### PLAN NO 10: EXISTING ACCESS INTO THE SITE



#### LEGEND

	Site Boundary		Areas to which motor vehicles currently have access
	Pedestrian Access		Pedestrian Crossing
	Current Vehicle Access		

#### **4.7 Quality of the External Spaces and Movement within the Site**

- 4.7.1 Plan 11 shows the location of external spaces on the site. Most of the external space on the ground level of the site is given over to parking for motor vehicles though cycle parking stands have been introduced to some of these spaces as part of the University's plan to generally reduce car parking and increase cycle parking provision. None of this space can be described as a public realm though it is publicly accessible. The David Attenborough Building was designed, as was typical at the time, with a raised public podium to separate the public- pedestrian realm from the service areas on the ground level. This was not successful due to the lack of connectivity and continuity with the wider city and to the lack of facilities on the podium.
- 4.7.2 Circulation within the site is chaotic due to the piecemeal development, the ad hoc nature of the external spaces and the prominence given to motor vehicles. Wayfinding is very difficult and public access to the Museums is confusing. Current work to the David Attenborough Building is improving this through the construction of a new stair up to the podium and a new external stairs from Corn Exchange Street.
- 4.7.3 Access and movement around the site is particularly poor for those with limited mobility due to the presence of motor vehicles though the site is fundamentally level. Again, improvements are being made through the works to the David Attenborough Building.

# THE NEW MUSEUMS SITE, DEVELOPMENT FRAMEWORK SPD

## THE EXISTING SITE

### PLAN NO 11: EXISTING EXTERNAL SPACES



#### LEGEND

- Site Boundary
- External Open Space
- External Open Space at Elevated Level



#### **4.8 Landscape Features and Ecology**

- 4.8.1 As can be seen from Plan 12, there are no significant landscape features on the site other than a small area of garden at the northern end of Free School Lane outside the Old Cavendish Laboratory. Within the interior of the block there is a self-seeded tree at the southern end of the Mond Building and some planting, including a Birch tree, in a raised bed adjacent to the Old Physical Chemistry Building. None of this has high ecological value. The work to the David Attenborough Building includes a green roof designed to encourage biodiversity on the site and an area of planting with a tree on the raised podium outside the new entrance foyer to the Museum of Zoology, and on Corn Exchange Street. Bird boxes, bat boxes and 'insect hotels' are also included in these works.

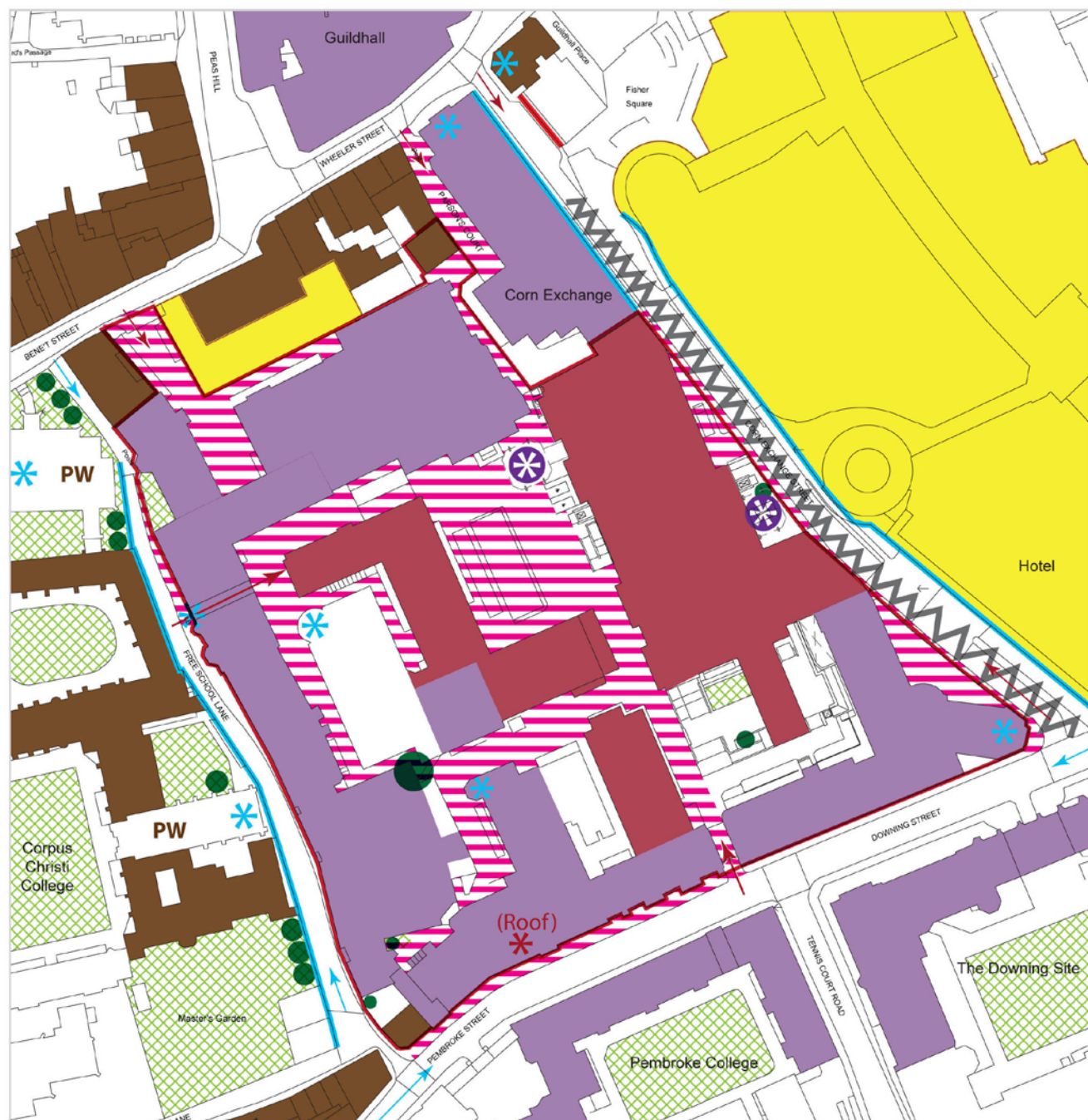
#### **4.9 Townscape**

- 4.9.1 The quality of the public realm within the site, as illustrated in Plan 12, has been identified as a key issue throughout the latter half of the 20<sup>th</sup> century to the present day. The problem is the piecemeal way in which the interior of the site developed with buildings erected as needs and budgets were identified rather than to conform to any particular plan.
- 4.9.2 Some of the buildings within the site are rather utilitarian, whilst others have positive features, but in many respects, the problem is how the buildings relate to each other. The quality of the spaces is extremely poor with many buildings extended or modified to the rear to accommodate all the necessary plant, flues, ducting and fire escapes, leaving little intervening space.
- 4.9.3 The space which does remain is dominated by car parking and utilitarian tarmacadam surface treatments to allow vehicular access, at the expense of pedestrians and cyclists. Soft landscaping within the site is limited to the small raised courtyards outside the Zoology Museum, the tree adjacent to the Balfour Building, some shrubs in the south west corner of the site and the area of planting on Free School Lane outside the Raleigh Wing. The settings of the heritage assets and the interior of the site's contribution to the conservation area is therefore extremely poor.
- 4.9.4 The exterior of the site, the south and west-facing street frontages, however contribute hugely to the townscape character of Free School Lane and Pembroke/Downing Street. The buildings of the site are an integral part of the townscape of these streets, complementing the buildings of the Downing Site and Pembroke and Corpus Christi Colleges. This is slightly marred by the large box addition to the roof of the former Chemical Laboratories. In common with the interior of the site, soft landscaping is limited, and there is only a small patch of shrubs outside the Rayleigh Wing, reputedly the last remnant of the original Botanic Garden.

# THE NEW MUSEUMS SITE, DEVELOPMENT FRAMEWORK SPD

## THE EXISTING SITE

### PLAN NO. 12 TOWNSCAPE QUALITY



#### KEY

	New Museums Site		Positive local feature		City Landmark building
	Negative floorscape		Negative local feature		Tree
	Domestically scaled traditional buildings (2-3.5 stories approx.)		Negative view		Place of worship
	C20 University buildings		Positive view		
	C20 Large scale commercial development		Negative frontage		
	Traditional institutional scale buildings		Positive frontage		
	Green space		Barrier to movement		

4.9.5 The site's contribution to the north, to Bene't Street and Wheeler Street (including Parson's Court) is far more limited as the site has no direct frontage onto these routes except for No.4 Parson's Court. Only poor quality glimpsed views are possible of the site's buildings from these streets.

4.9.6 The site's contribution to the east, to Corn Exchange Street, is entirely generated by the monumentality of the David Attenborough Building which contributes to the 'service route' character of this street.

#### **4.10 Roofscape**

4.10.1 At roof level, aside from the towers of the David Attenborough Building, the site does not feature strongly in the city's skyline, and there are limited points outside the site from where it is seen at a raised level.

4.10.2 In street views, the traditional, largely Victorian/Edwardian properties of Free School Lane and Pembroke/ Downing Streets with their variety of pitched roof forms contributes positively to the character of these streets. The notable exception is the large later 'box' addition on top of the Old Metallurgy Building, which hides the attractive ventilation shaft of this building.

4.10.3 Along Pembroke/Downing Streets, the variety of pediments that break the strong parapet line is a particular feature of the built form on this route, and echoes that of the Pembroke College buildings on the opposite side of the road. The stepped gables of the remodelled Perse School and the cupola of the old hall within this complex are attractive features of Free School Lane's roofscape, complemented by the use of half-dormers on the Old Cavendish Laboratory.

4.10.4 Views across the site from raised points nearby, including Great St Mary's Church tower and the Grand Arcade multi-storey car park, show the variety of roof forms within the site. Chimneys, skylights, attic windows and a mixture of gabled/hipped roofs are prevalent amongst the traditional properties, whilst the later 20<sup>th</sup> century buildings have either flat roofs or mansard roofs and a large number of flues or other paraphernalia. However, dominating the eastern part of the site is the considerable mass of the David Attenborough Building.

4.10.5 In common with much of the historic core, the prevailing feature of the site's roofscape is its variation, a consequence of 150 years of continual development. It diverges between single storey to eleven storeys and traditional pitched roof forms and materials to flat roofs; all across one city block of approximately 1.5ha.

#### **4.11 Infrastructure**

- 4.11.1 The infrastructure on the site and the servicing of individual buildings on the site has developed in a piecemeal fashion over time. The systems are therefore complex, confused and inefficient and as a result very difficult to maintain and adapt.
- 4.11.2 Energy use is high due to the nature of the various buildings on the site.
- 4.11.3 There is no attenuation of storm water drainage.

#### **5.0 PARAMETERS FOR CHANGE**

- 5.0.1 This section of the SPD identifies the constraints and opportunities that will shape how future development of the NMS will take place.
- 5.0.2 A total of four key “parameters” are set out here. They include connectivity and movement; open space and the public realm; built form; and sustainability. Proposals on the NMS site should be in broad compliance with the parameters as set out herein.

#### **5.1 Connectivity and Movement**

##### **a. The Urban Block**

- 5.1.1. The NMS is the larger part of an existing urban block which has been formed through the long historical development of the city. The block makes an essential contribution to the Central Conservation Area through its definition of urban structure and through the architectural quality and variety of the frontages of the buildings around the perimeter, particularly along Downing Street and Free School Lane. Its fundamental form and identity should remain unchanged.
- 5.1.2 However, the block fails to contribute to the wider context in two crucial respects. Firstly its perimeter is very closed and does not engage with the surrounding streets; and secondly, the interior of the site provides no clear routes or open spaces of any quality to contribute to the grain and continuity of the wider public realm.

b. Urban Connectivity

- 5.1.3 Opportunities should be explored to improve the quality of the public realm outside the boundaries of the site and to better the relationship of the NMS to the wider city centre more generally and to the University's other city centre sites more specifically. This will require consideration of improvements to the surrounding streets as part of the public realm and as part of the city centre traffic management plans. Cambridgeshire County Council, on behalf of the City Council and other stakeholders, entered into the City Deal with central government in 2014 to help deliver economic benefits through improvements to transport infrastructure in and around the city. Along with improvements to the public realm which can be made by the University of Cambridge, the City Deal offers a potential opportunity to improve wider accessibility of the city centre to the benefit of the NMS site.
- 5.1.4 A recent study known as the Cambridge City Centre Access Study – DRAFT (January 2015) prepared by Beacon Planning and led by the City Council also recommended areas for improvement within and around the city centre. Recommendations from this study will be considered further in respect of how improvements in the surrounding public realm can provide mutual benefit to the NMS and the public using surrounding streets. The report and relevant council meeting minutes (Community Services Scrutiny Committee, 19 March 2015) related to this study may be found at:
- <http://democracy.cambridge.gov.uk/ieListDocuments.aspx?CId=176&MId=2574&Ver=4>
- 5.1.5 Corn Exchange Street is the vehicle entrance and exit to the main city centre car park which is part of the Grand Arcade. If it were possible, this location would benefit from increased areas of pavement in order to improve the safety of pedestrians, (such as those being implemented at the time of writing as part of the works to the David Attenborough Building), but it is unrealistic to imagine it becoming a principal place of arrival for pedestrians or for it to be more than the service road it has become.
- 5.1.6 Free School Lane on the other hand is a unique and high quality part of the Central Conservation Area. It provides a pleasant and enjoyable place in which to experience many Listed Buildings and the scale of a medieval street with views of St Benet's Church and King's College Chapel beyond. The entrance to the Whipple Museum however, though charming in its scale, is unsatisfactory in terms of access. Opportunities to address this issue and to make the Museum more visible should be explored.
- 5.1.7 Wheeler Street, to the north, is a busy and energetic part of the city full of character with a number of retail units, public houses and St. Benet's Church. The NMS faces onto the street with a nondescript yard currently used as a car

park and service area. This should be improved so that the University's presence in the city is made more visible and the street scene improved, in this case especially from Bene't Street.

- 5.1.8       Pembroke Street and Downing Street form a complex place which is an important part of the city centre, with many buildings of high quality and its own particular spatial and architectural character. Four of the University's internationally important museums are located along the street, as well as one of the largest hotels in the city, and it forms a key link between the mostly University and College focussed area along Trumpington Street and the commercial area along St Andrews and Regent Streets.
- 5.1.9       Pembroke and Downing Streets are, however, dominated by vehicular traffic flows resulting from the current city centre traffic system, with particularly congested junctions at the intersections with Trumpington Street and with Tennis Court Road.
- 5.1.10      A future, wider re- consideration of city centre traffic movement might address the various issues posed by this problem but, in the specific context of the NMS, it is the area defined by the existing entrance loggia to the Downing Site to the north and east, and the junction with Free School Lane to the south and west, that is crucial.
- 5.1.11      The adoption of a less conventional approach to the design of the highway in this area and the prioritisation of pedestrians would not only facilitate better and safer movement between the sites for the large numbers of members of the University who travel between them, but would also better connect the Museums and create opportunities for them, and the University generally to engage in the wider public realm. Such a space would need very careful design to make drivers aware of the changed priorities and their responsibilities. The design of the paving, the introduction of trees and a system of controlling traffic queuing for the car park during busy periods could all potentially play a part in this.
- 5.1.12      There is a potential opportunity to introduce small specialist retail outlets at ground level along Pembroke Street which would help activate this street scene. The appropriateness of any outlets would, however, need to be tested at a detailed level, in particular in respect of appropriateness of fit with the heritage assets and accessibility in this part of the site.

c. Site permeability

- 5.1.13      An increase in the permeability of the site for pedestrians is critical and must be delivered as part of the redevelopment of the site. This can only be achieved through the creation of new entrances, and, as identified on Plan 13 specific opportunities which exist are as follows:



- Pembroke Street. Pembroke Street is the main and busiest public street relating to the NMS and as such a primary public entrance, or 'front door', to the site is required. The street frontage is an important part of the central conservation area but sensitive and creative architectural design could engage with the existing buildings to make this frontage more permeable at ground level.
- Off Bene't Street through the yard onto which the Arts School faces. (Bene't Street Yard). This existing yard is currently used as a car park and the view into it from Bene't Street is terminated by the almost blank and very plain back wall of the Cavendish Laboratory. Though this is a Listed Building, existing window openings could be sensitively adapted and possibly enlarged to provide a new entrance with a view into the site in association with general environmental improvements to the yard and the way it engages with the street.
- Free School Lane. The current public entrance to the Whipple Museum is unsatisfactory for both members of the public and the Department of the History and Philosophy of Science in which it is located. Access for disabled and those visiting as groups is via a lift which is located at the rear of the building accessed from within the site itself. If done with sensitivity to the existing buildings in this location, an opportunity exists in the notional gap between the geometries of the Old Physical Chemistry Building and the Phoenix Building to create a new entrance leading into a new foyer. This would separate the public and department entrances and improve disabled access while at the same time forming a new entrance to the site as a whole.

There are limited opportunities for improvement to the existing access way to the site via Parson's Court at the northern end of the site due to its use as a spill out space for the technical and ancillary areas of the Corn Exchange, though this route will be maintained.

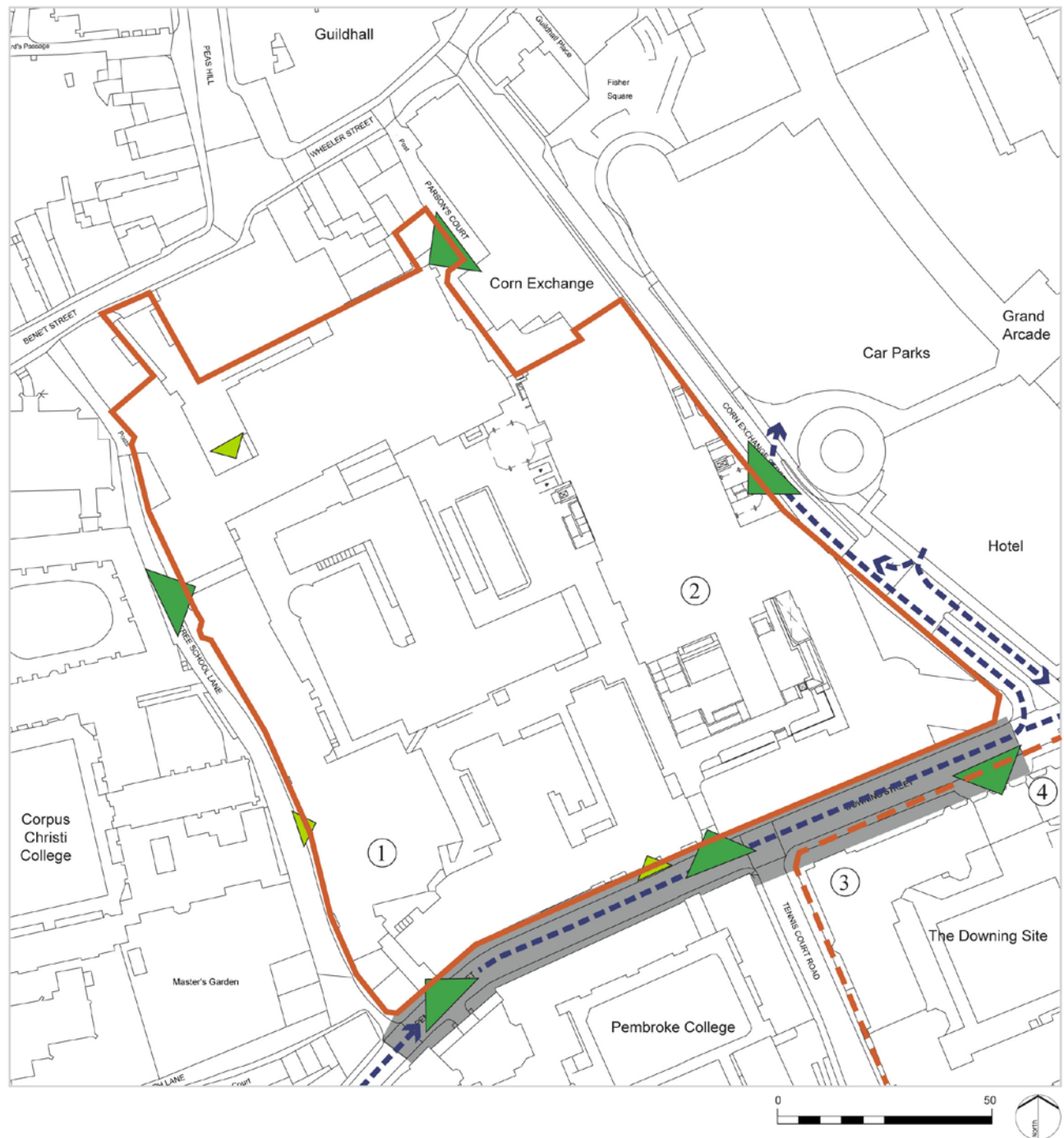
d. Vehicular Access

- 5.1.14 Access to the site by motor vehicles shall be limited to the minimum requirements to meet those needs which cannot be met in any other way. Access control mechanisms will ensure that motor vehicles do not casually enter the site without prior permission. Movement within the site will be limited to have a minimum impact on the layout and quality of the public spaces with parking constrained to those areas adjacent to the vehicular access points. The proposed arrangement of managing motor vehicles is shown on Plan 14.

# THE NEW MUSEUMS SITE, DEVELOPMENT FRAMEWORK SPD

## DEVELOPMENT FRAMEWORK

### PLAN NO 13: ACCESS, ENTRANCES & PUBLIC REALM OUTSIDE THE SITE BOUNDARY



#### LEGEND

- New Museums Site Boundary
- New Museums Site Boundary
- ▲ Existing site entrance
- ▲ Approximate location for new site entrances

- Area of public realm improvements
- ➡ Primary traffic route

#### University Museums

- 1 Whipple Museum
- 2 Zoology Museum
- 3 Museum of Archaeology & Anthropology
- 4 Sedgwick Museum of Earth Sciences

e. Service Vehicles

- 5.1.15 Service vehicles will as a matter of course be directed to the layby and goods in/out facility on the lower ground floor of the David Attenborough Building on Corn Exchange Street. Specialist service vehicles will be permitted to enter the site when necessary by prior arrangement.
- 5.1.16 Waste from the various University institutions on the site will be stored in a central location from where it can be removed by specialist vehicles accessing the site via the Pembroke Street arch.

f. Parking for the disabled

- 5.1.16 Disabled parking spaces will be provided in both Bene't Street Yard and at the entrance to the site from Pembroke Street.

g. Emergency Vehicles

- 5.1.17 Access to the site for emergency vehicles will continue to be limited by the size of the existing entrance through the archway leading from Pembroke Street. The movement of a fire tender able to enter the site through the existing arch around the site will be facilitated to allow fire-fighting. Dry horizontal 'risers' will be provided where it is not possible to provide tender access.

h. Shared Use Cars

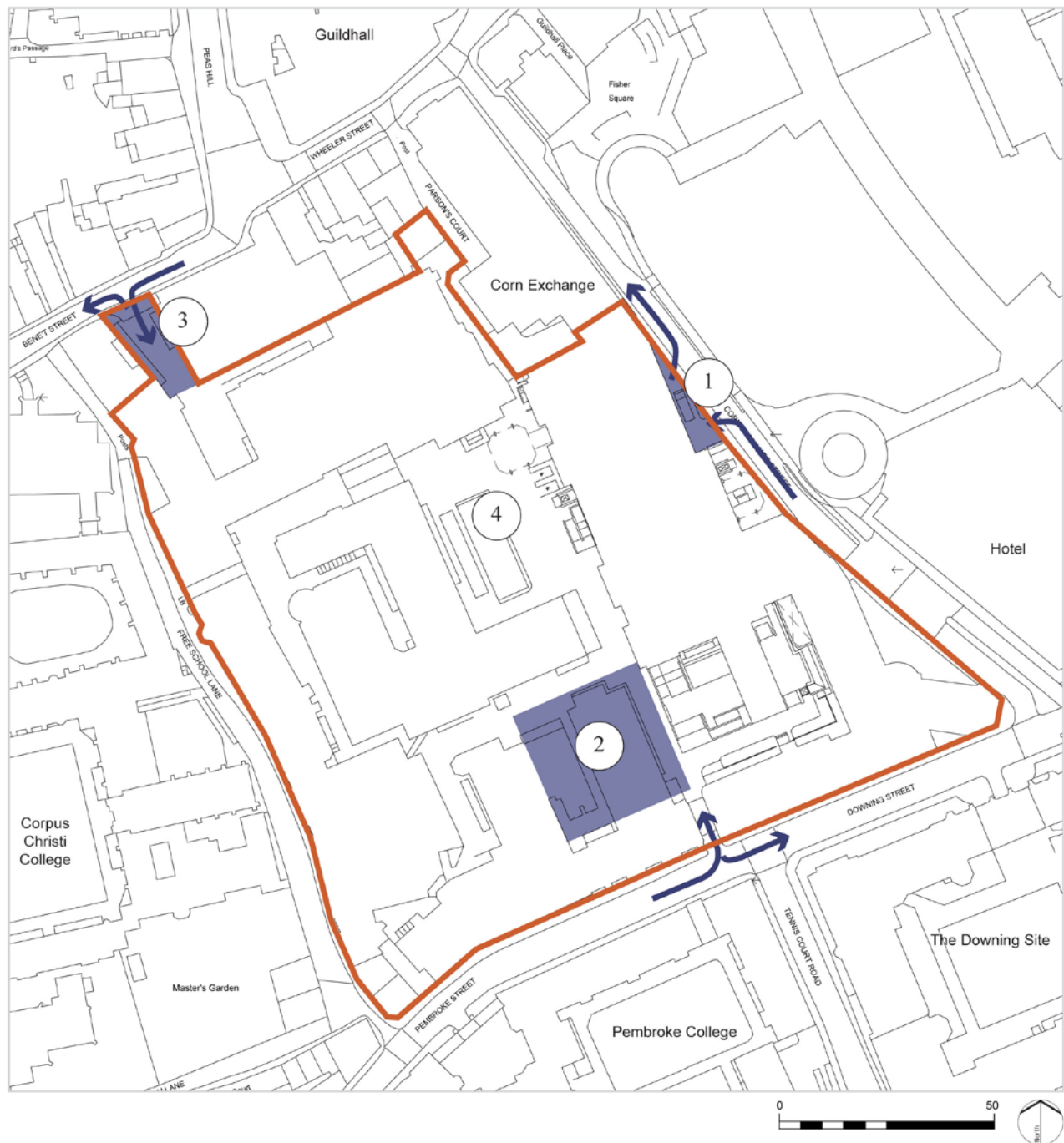
- 5.1.18 A small number of parking spaces for University vehicles provided for the use of staff on essential business and available via a central booking system will be provided near the main vehicle entrance from Pembroke Street.

i. Bicycles








- 5.1.19 Access and parking facilities for bicycles will be improved. Parking facilities will be provided in a manner such that they do not dominate and overpower the open spaces. Underground parking will be provided where possible either in existing basements or through the use of proprietary systems with clusters near the main entrances. Imaginative and creative solutions to this issue will be encouraged.
- 5.1.20 Ancillary facilities such as showers, lockers and basic maintenance facilities (compressed air) should be provided at key points and where possible in new development.

# THE NEW MUSEUMS SITE, DEVELOPMENT FRAMEWORK SPD

## DEVELOPMENT FRAMEWORK PLAN NO 14: MANAGEMENT OF MOTOR VEHICLES



### LEGEND

- |                                                                                                                                   |                                                                                                                                               |
|-----------------------------------------------------------------------------------------------------------------------------------|-----------------------------------------------------------------------------------------------------------------------------------------------|
|  Site Boundary                                 |  1 Service Layby                                           |
|  Areas of the NMS with access by motor vehicle |  2 Disabled parking, specialist service and waste vehicles |
|  Direction of travel                           |  3 Disabled parking                                        |
|                                                                                                                                   |  4 Emergency vehicles only                                 |

## 5.2 Open Space and the Public Realm

### a. Public Realm within the site

- 5.2.1 The creation of high quality open spaces within the site is also a priority. These spaces should provide the clarity, cohesion, continuity and spatial identity for the site which it is currently lacking. They should also provide more appropriate settings for new buildings and the Listed Buildings on the site and make the heritage of the site more accessible and visible.
- 5.2.2 Opportunities exist for three new main primary open spaces (referred to as A, B and C respectively on Plan 2: Illustrative Masterplan and as shown on Plan 15 below):
- An entrance court on the site of the existing Shell Building. This would be the main reception space to the site accessed from Pembroke Street. It should be a place of welcome and orientation and should be able to cope with the numbers of people and groups of people visiting the site and the Museums on it. The foyer to the Museum of Zoology and the café on the podium of the David Attenborough Building (under construction at the time of writing), as well as the foyer of the Whipple Museum, should address and be accessed from this space. The space should also have a scale appropriate to this part of the site as determined by the monumental character of the David Attenborough Building and the frontages of the buildings facing onto Downing Street and Pembroke Street.
  - A central court in the existing area between the David Attenborough Building, Cockcroft, Austin and Examinations Hall buildings. This should be the heart of University life on the site and form an open air 'foyer' to the Babbage Lecture Theatre, which is one of the two large University lecture theatres in the city centre, and the Examinations Halls. The space should also be used to create terraces or steps that could mediate between the ground level and the raised podium of the David Attenborough Building, and mitigate its unwelcoming face at ground level.
  - A third court. A welcoming open space, reached and visible through the existing archway from Free School Lane and a possible new entrance from Bene't Street, could be created to form a new and appropriate setting for the entrance drum to the Mond Building. The character of this space should reflect the smaller scale of the buildings and urban grain in this part of the city.
- 5.2.3 The site is the property of the University and as such access to the site, including the open spaces, will be controlled for reasons of security and safety. In general however, they will be open to the public and will form a part of the public realm of the city. Even spaces such as the Babbage Lecture Theatre, which will generally be private spaces for University teaching, will

on occasion become a part of this public realm at certain times when, for example, they become venues for public lectures; a function that will be encouraged and facilitated by the redevelopment.

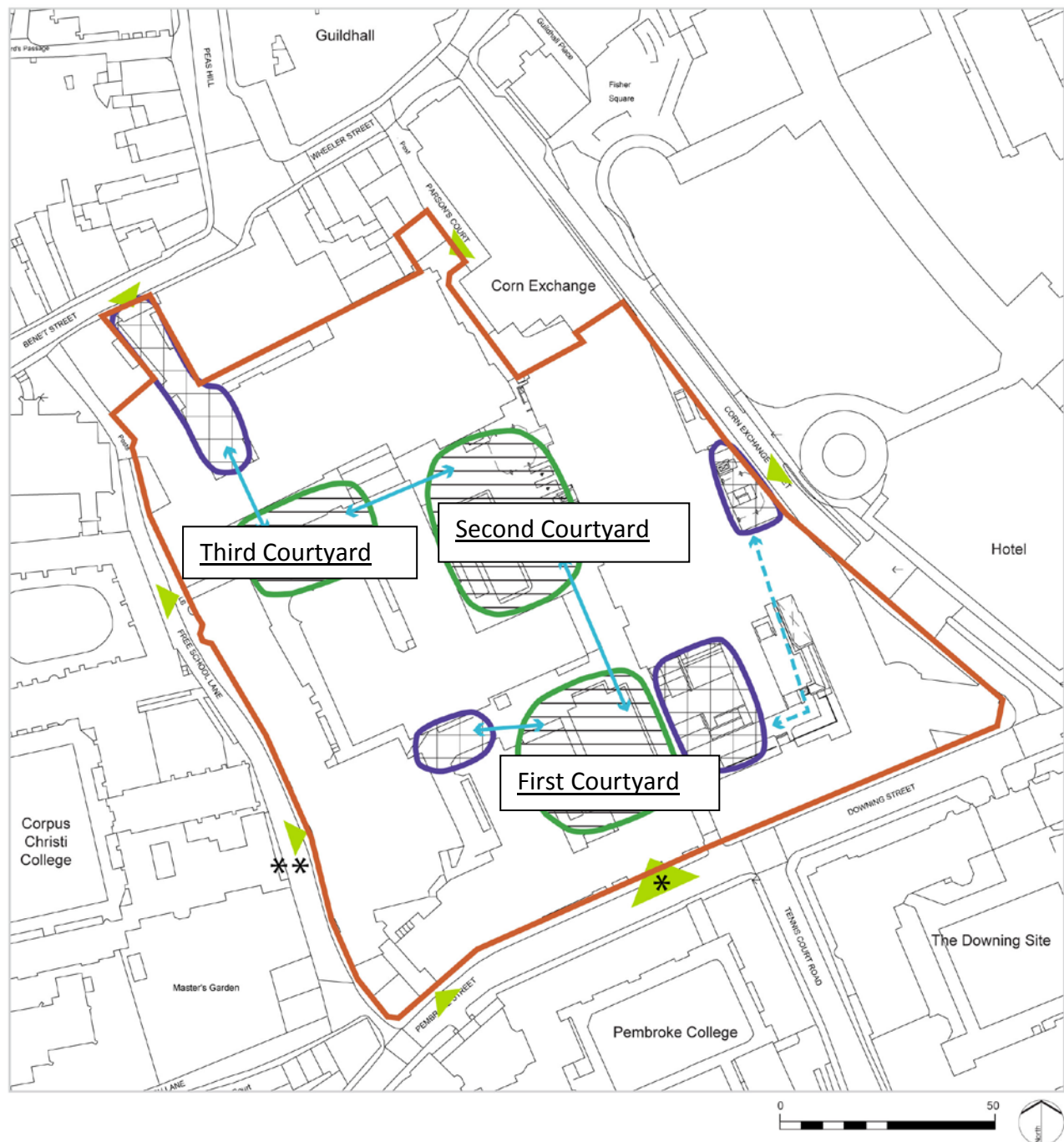
- 5.2.4 Both open and enclosed secondary and tertiary spaces, of a smaller scale and less public nature, should also be created as a continuation of the public realm, thereby spatially linking institutions within the University and providing variety to the grain and scale of the townscape.
- 5.2.5 The design of open spaces should be developed to relate and give structure to existing facilities and buildings. Consistent specification of paving materials, signage, furniture and fittings and the detailed design of build elements such as steps, ramps, raised planters and tree pits should allow for a coherent approach to open spaces across the site.
- 5.2.6 The new open spaces should be planted with trees in such a way that they contribute to the spatial structure of the place. The areas underground should be used for storm water attenuation and surface water features relating to this, which would make an important contribution to the quality of the environment, should be incorporated where possible.
- 5.2.7 External lighting should be discrete to avoid light pollution and optimise energy use but should nevertheless fulfil its functions in creating a safe and legible environment through the illumination of key features such as entrances, art work, primary pedestrian routes and vistas.








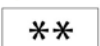

# THE NEW MUSEUMS SITE, DEVELOPMENT FRAMEWORK SPD

## DEVELOPMENT FRAMEWORK

### PLAN NO 15: PROPOSED OPEN SPACE & THE PUBLIC REALM



#### LEGEND

	Site Boundary		Public entrance to the site
	New areas of open spaces		Main public entrance to the site
	Pedestrian routes between open spaces		Public entrance to the site - via Whipple Museum foyer
	Existing area of main open space upgraded		

b. Public Art

- 5.2.8 Public art should be delivered across the site in a holistic and coherent way and a strategy to deliver this will be submitted with the first major proposal for redevelopment.
- 5.2.9 Public art should engage with the site's heritage and its continuing importance to public life and knowledge.
- 5.2.10 Public art should contribute to other important issues addressed in this SPD such as the quality and cohesion of the public realm, the creation of comprehensible and high quality external spaces, public engagement and interpretation of heritage.

c. Heritage

- 5.2.11 The site has an extraordinarily rich history but this is largely invisible or inaccessible except for the presence a number of wall plaques. The heritage embedded within the site, including its archaeology; use as a botanic garden; contributions to the history of science and the architecture of special interest, will be made more accessible by the creation of a significant public realm, as described in previous sections, and through public art and information systems including technologies such as smart phones and Wi-Fi. The history of the University as well as the current and future contributions being made by those working on the site will also be made more accessible in this way.

**5.3 Built Form**

a. Existing Buildings

- 5.3.1 The existing buildings on the site have suffered from the need to adapt them to modern functional requirements for laboratories and other specialist uses over a long period of time. The University's initiatives in relocating the science orientated departments that have a need for such facilities to West Cambridge and for re-using the buildings as accommodation for the Humanities and Social Sciences, which are largely desk based activities, provides an opportunity to remedy the situation.
- 5.3.2 The plethora of plant mounted on the rear elevations of the buildings, for example, should be addressed as should inappropriate roof top additions and the current situation in which buildings around the perimeter of the site now turn their backs to their surroundings as a result of functional needs within, with many blacked out windows. Creative adaptation and restoration of the interiors of these buildings would improve their contributions to the public realm outside and is to be encouraged. Opportunities to improve the

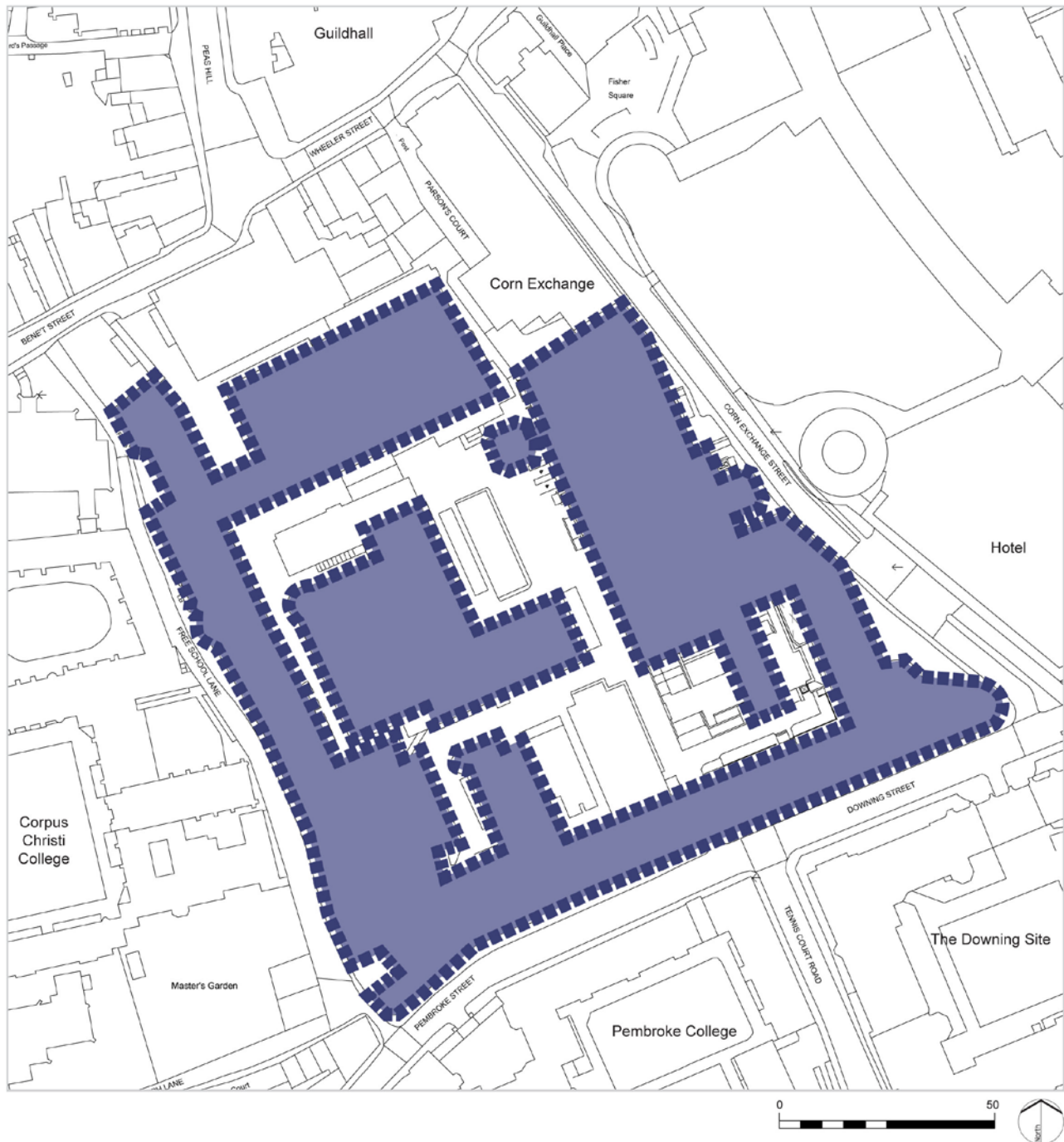
relationship of the brick plinth of the David Attenborough Building to its surroundings should be explored.

b. New Buildings

- 5.3.3 Existing buildings will continue to determine the overall visual character of the site but the creation of new open spaces and the introduction of new facilities within new buildings within the site will require the demolition of some structures and, in turn, the re-provision of useable space elsewhere on the site.
- 5.3.4 Opportunities also exist for new buildings to replace existing buildings of a lesser quality. These are primarily in the centre of the site where much ad hoc development took place in the mid-twentieth century to meet the urgent space needs of the Dept. of Physics and where the contribution of the townscape to the Historic Core Conservation Area is negligible or non-existent.
- 5.3.5 New buildings should be considered firstly in relation to the structure of the external spaces and the need to provide natural light and sunshine into the interior of the site. The height of buildings in this respect is of key importance and proposals for new buildings will need to demonstrate their impact on the quality of the external spaces through daylight and sun path modelling. New buildings must also be of a height appropriate to the historic context of the site and not “compete” with existing buildings and the site in its immediate and wider townscape.
- 5.3.6 Entrances should be orientated towards the public spaces and ground floors should contribute to the activity and life of the public realm where appropriate.
- 5.3.7 Roofs should be designed to add to the grain and variety of the city roofscape and roof top plant should be avoided if possible. Any new roof plant should be positioned discreetly on roof tops and screened appropriately.
- 5.3.8 New buildings on the site should be designed to a high standard and to address contextual issues including scale, size, the use of materials and the way in which openings in facades are formed.
- 5.3.9 The proposed built form is annotated with the existing and proposed building footprints on Plan 16.

# THE NEW MUSEUMS SITE, DEVELOPMENT FRAMEWORK SPD

## DEVELOPMENT FRAMEWORK PLAN NO 16: PROPOSED BUILT FORM



### LEGEND



Approximate outline of built form

Note 1: Includes existing and future building frontages  
Note 2: Building alignment/ outline is indicative only

## **5.4 Sustainability**

### **a. Sustainability**

5.4.1 Creating a sustainable place within the city should be a priority underpinning all initiatives to redevelop the site and a holistic and site wide approach, addressing environmental, social and economic issues is required. These should include;

- i. Health and well-being of occupants and visitors
- ii. Energy efficiency of new buildings
- iii. Design for climate change
- iv. Water use
- v. Flood mitigation
- vi. Use of materials and resources
- vii. Waste
- viii. Employment opportunities
- ix. Pollution
- x. Transport and mobility
- xi. Conservation of heritage assets in a manner appropriate to their significance

5.4.2 Specific initiatives to be included are

- i. A Combined Heat and Power network driven from a site wide energy centre
- ii. A site wide storm drainage scheme to attenuate storm water run-off from the site is to be implemented using green roofs and storage of water below open spaces
- iii. Reuse of existing structures and materials arising from the works itself
- iv. Specification of materials with a view to social and environmental impacts
- v. Initiatives and infrastructure in support of sustainable modes of travel
- vi. Technologies, especially photovoltaics to generate renewable energy where appropriate on new roofs
- vii. Improved infrastructure reducing risks of pollution and noise
- viii. Design of new buildings using passive environmental control principles, natural ventilation and natural daylighting
- ix. Improved air quality and decrease in noise pollution through the management of vehicles and plant
- x. Conservation of heritage assets in a manner appropriate to their significance

5.4.3 Though standard industry-wide assessment methods should be used where appropriate (such as in the construction of new buildings for example), a less rigid approach should be considered with regard to existing buildings and the site as a whole. Specific issues should be addressed and benefits sought though the setting of real and challenging targets and through the monitoring

of achievements against those targets rather than through the application of prescribed criteria, which might not be relevant in this unique location. Other sections of this document are also relevant to the wide agenda of sustainability.

b. Ecology

- 5.4.4 There are many opportunities for the ecology and biodiversity of the site which currently has very low ecological value. Initiatives should be developed in the context of wider plans for improvements to the ecology and biodiversity of the city centre and to locations situated within Cambridgeshire. Such initiatives would be particularly appropriate given the site's history as the University Botanic Garden.

These should include the provision of;

- i. Trees and planting throughout the site where appropriate
- ii. Water resources at both ground and higher levels ( in association with SUDS and landscape features where possible)
- iii. Nesting opportunities for a variety of bird and bat species
- iv. Habitats for insects

- 5.4.5 New and existing flat roofs provide a specific opportunity to improve the ecology of the site and to contribute to the general increase in the biodiversity of the city centre. The creation of calcareous grasslands, which are a feature of the region, should be considered together with neutral grassland environments. A variety of substrates and microclimates have a part to play and both green and brown roofs, which support much needed habitats for invertebrates, are desirable. The installation of photovoltaics on roofs should not prohibit the creation of planted roofs as the shade will add variety to the environmental conditions.

- 5.4.6 Opportunities for the creation of green walls comprising climbing plants should be explored in north and east facing walls to provide nectar sources for invertebrates and cover and night roosting sites for birds.

- 5.4.7 Initiatives that link the provision of ecological enhancements with the public art strategy should be explored.

c. Infrastructure

- 5.4.8 Infrastructure on the site should be improved where possible through rationalisation and consolidation of services in order to ensure fitness for purpose and minimisation of disruption in the future.



## **5.5 Phasing and Implementation**

- 5.5.1 The New Museums Site is home to a number of University departments and Museums, and learning and teaching spaces, and most of the site will remain in operational use as development comes forward. It is likely that change will be delivered over a number of years and phases. The Masterplan identifies discrete areas on the site where development is likely to come forward at the same time.
- 5.5.2 The phasing of development of these areas will need to be determined with detailed reference to the specific nature of the existing conditions, the proposals for change and the need to maintain a reasonable environment for those continuing to travel, live and work in the vicinity.
- 5.5.3 It is essential that each phase is approached as part of the larger redevelopment of the site and that design is focused on the quality and coherence of the site as a whole.

## **6. PLANNING OBLIGATION**

- 6.1 As part of improving the immediate public realm around the site, the University should provide a contribution to the upgrading to those parts of Downing Street near the revised entrance to the site. Such improvements are necessary also to help improve the site relationship with the entrance opposite to the Biocentrum site, also known as the Downing Site, also a University facility. Agreement will be necessary with Cambridgeshire County Council on the design and implementation of such measures.

## **APPENDIX A - GLOSSARY OF TERMS**

### **Active frontages**

An active frontage is one which allows some kind of movement or visual relationship between the person outside and the activity inside. At a minimal level, this interaction might be one of simple observation such as a window display or people working. At a higher level of interaction, the pedestrian could be encouraged to enter the unit to buy something or participate in an activity. The most interactive frontages are usually those of cafés, bars or shops, which spill out onto the street.

### **Accessibility**

The ease with which a building, place or facility can be reached by people and/or goods and services. Accessibility can be shown on a plan or described in terms of pedestrian and vehicle movements, walking distance from public transport, travel time or population distribution.

### **Articulation**

The expression of the vertical or horizontal subdivision of a building facade into perceivable elements by the treatment of its architectural features.

### **Barrier**

An obstacle to movement.

### **Biodiversity**

Encompasses all aspects of biological diversity, especially including species richness, ecosystem complexity and genetic variation.

### **Biodiversity Action Plan (BAP)**

A plan that sets objectives and measurable targets for the conservation of biodiversity.

### **Block/Urban Block**

The area bounded by a set of streets and undivided by any other significant street.

### **Building element**

A feature (such as a door or window) that contributes to the overall design of a building.

### **Building line**

The line formed by the frontages of buildings along a street.

### **Built form**

Buildings and their structures.

### **Bulk**

The combined effect of the arrangement, volume and shape of a building or group of buildings. Also called massing.

### **Buildings of Local Interest**

Buildings of Local Interest are not subject to statutory protection, but are recognised as being of importance to the locality or the City's historical and architectural development.

### **Cambridge Local Plan 2006**

The Cambridge Local Plan 2006 sets out policies and proposals for future development and land use to 2016; the Plan will be a material consideration when determining planning applications.

### **City Centre**

Historic Core and Fitzroy/Burleigh Street shopping areas in Cambridge. These areas provide a range of facilities and services, which fulfil a function as a focus for both the community and for public transport. See also Cambridge Proposals Map (February 2008).

### **Conservation Areas**

Areas identified, which have special architectural or historic interest, worthy of protection and enhancement.

### **Desire Line**

An imaginary line linking facilities or places which people would find it convenient to travel between easily.

### **Enclosure**

The use of buildings to create a sense of defined space.

### **Eyes on the street**

People whose presence in adjacent buildings or on the street make it feel safer.

### **Facade**

The principal face of a building.

### **Fenestration**

The arrangement of windows on a facade.

### **Fine grain**

The quality of an area's layout of building blocks and plots having small and frequent subdivisions.

### **Form**

The layout (structure and urban grain), density, scale (height and massing) and appearance (materials and details).

### **Habitats Regulation Assessment**

An assessment of the potential effects of a proposed plan in combination with other plans or projects on one or more European sites, Special Areas of Conservation, Special Protection Areas and RAMSAR sites). Required by the Habitats Directive 92/43/EEC, this assessment

must be carried out for all local development documents, including SPDs, and approved by Natural England prior to the adoption of the document in question.

**Legibility**

The degree to which a place can be easily understood by its users and the clarity of the image it presents to the wider world.

**Listed Building**

A building or structure of special architectural or historic interest and included in a list, approved by the Secretary of State. The owner must get Listed Building Consent to carry out alterations that would affect its character or its setting.

**Local Biodiversity Action Plan (LBAP)**

The Action Plan works on the basis of partnership to identify local priorities and to determine the contribution they can make to the delivery of the national Species and Habitat Action Plan targets. The Local Biodiversity Action Plan has been prepared by Biodiversity Cambridgeshire (contact via Cambridgeshire County Council) 1999.

**Local Plan**

Abbreviation used to describe the statutory plan adopted by the City Council. It is a material consideration in determining planning applications, which should be in accordance with them as part of the Development Plan.

**Major Development**

Defined as:

Residential development: the erection of 10 or more dwellings or, if this is not known, where the site is 0.5 hectares or more; or other development: where the floor area to be created is 1,000m<sup>2</sup> or more, or the site area is 1 hectare or more.

**Massing**

The combined effect of the arrangement, volume and shape of a building or group of elements. This is also called bulk.

**Mitigation**

The purpose of mitigation is to avoid, reduce and where possible remedy or offset any significant negative (adverse) effects on the environment etc. arising from the proposed development.

**Movement**

People and vehicles going to and passing through buildings, places and spaces.

**Natural surveillance**

The discouragement to wrong-doing by the presence of passers-by or the ability of people to see out of windows. Also known as passive surveillance.

**Open Space**

Includes all open space of public value. There is a broad range of spaces that may be of public value - not just land but also areas of water such as rivers and lakes - and includes,

parks and gardens; natural and semi-natural urban greenspaces; green corridors; outdoor sports facilities; amenity greenspace; teenager's and children's play areas; allotments and community gardens; cemeteries and churchyards; accessible countryside in urban fringe areas and civic spaces.

### **Parking Standards**

Document setting out maximum permissible levels of car parking for various use-classes, along with minimum levels of cycle parking.

### **Permeability**

Permeability describes the degree to which urban forms, buildings, places and spaces permit or restrict the movement of people or vehicles in different directions. Permeability is generally considered a positive attribute of urban design, as it permits ease of movement by different transport methods and avoids severing neighbourhoods. Areas which lack permeability, e.g. those severed by arterial roads or the layout of streets in cul-de-sac form, are considered to discourage effective movement on foot and encourage longer journeys by car.

### **Planning Condition**

Requirement attached to a planning permission. It may control how the development is carried out, or the way it is used in the future. It may require further information to be provided to the Local Planning Authority before or during the construction.

### **Planning Obligation**

A binding legal agreement requiring a developer or landowner to provide or contribute towards facilities, infrastructure or other measures, in order for planning permission to be granted. Planning Obligations are normally secured under Section 106 of the Town & Country Planning Act 1990.

### **Public Art**

Publicly sited works of art, which make an important contribution to the character and visual quality of the area and are accessible to the public. Details as per adopted Public Art SPD and any successor document.

### **Public Realm**

The parts of a village, town or city (whether publicly or privately owned) that are available, without charge for everyone to use or see, including streets, squares and parks.

### **Renewable Energy**

Renewable energy covers those energy flows that occur naturally and repeatedly in the environment – from the wind, the fall of water, the movement of the oceans, from the sun and from biomass.

### **Section 106**

See Planning Obligation.

### **Shared Space**

A street or place accessible to both pedestrians and vehicles that is designed to enable pedestrians to move freely by reducing traffic management features that tend to encourage users of vehicles to assume priority.

### **Spill out space**

Space used in association with an adjacent building (tables and chairs on the pavement outside a cafe, for example).

### **Supplementary Planning Guidance (SPG) /Supplementary Planning Document (SPD)**

SPDs add detail to policies laid out in development plan documents, or a saved policy in an existing development plan. These may take the form of design guides, area development briefs, a master plan or issue-based documents. These documents can use illustrations, text and practical examples to expand on how the authority's policies can be taken forward.

Local authorities must involve the community in the preparation of SPDs. They are also subject to a Sustainability Appraisal to ensure economic, environmental and social effects of the plan are in line with sustainable development targets.

The SPD may be taken into account as a material consideration in making planning decisions such as determining planning applications.

### **Sustainability Appraisal (SA)**

An appraisal against sustainability criteria of proposals.

### **Sustainable Development**

Sustainable Development is a very broad term that encompasses many different aspects and issues from the global to local levels. Overall sustainable development can be described as 'Development, which meets the needs of the present without compromising the ability for the future generations to meet their own needs' (after the 1987 Report of the World Commission on Environment and Development – the Brundtland Commission).

### **Sustainable Drainage Strategy (SuDS)**

Development normally reduces the amount of water that can infiltrate into the ground and increases surface water run-off due to the amount of hard surfacing used. Sustainable drainage systems control surface water run off by mimicking natural drainage process through the use of surface water storage areas, flow limiting devices and the use of infiltration areas or soakaways etc.

### **Transport Assessment (TA)**

The Assessment [or Consideration] of the potential transport impacts of a proposed development, with an agreed plan to reduce or mitigate any adverse consequences and where appropriate establish how more sustainable modes of travel can be increased.

### **Travel Plan**

Package of measures tailored to a particular site, aimed at promoting more sustainable travel choices (such as walking, cycling, public transport) and reducing car use. It may



include initiatives such as car sharing schemes, provision of cycle facilities, improved bus services, and restricting or charging for car parking.

**Urban Grain**

The pattern of the arrangement and size of buildings and their plots in a settlement; and the degree to which an area's pattern of street blocks and street junctions is respectively small and frequent, or large and infrequent.

**Use Class**

The Town and Country Planning (Use Classes) Order 1987 (as amended) established Use Classes, which is a system for classifying uses of land.

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## **New Museums Site Development Framework Supplementary Planning Document Statement of Consultation**

### **Background**

The purpose of the New Museums Site Development Framework Supplementary Planning Document (SPD) is to guide the redevelopment of the New Museums Site allocation, which is allocated in the Cambridge Local Plan 2014: Proposed Submission document (as amended<sup>1</sup>).

The SPD sets out the joint aspirations of the council and the University of Cambridge regarding future changes to the site. It will provide greater clarity and certainty, particularly in terms of:

- The needs for new and/or improved facilities;
- The clustering of associated University disciplines, in order to achieve academic benefits and administrative operating efficiencies;
- The provision of high quality, sustainable buildings and places;
- The delivery of optimum space efficiency.

On adoption, the SPD will be a material consideration in the determination of relevant planning applications.

The SPD has been written to support the Cambridge Local Plan 2014: Proposed Submission. This SPD will be adopted at the same time as, or shortly after, the local plan is adopted. It will not be adopted before the local plan is adopted.

### **Document Production**

The SPD has been prepared in collaboration with the University of Cambridge and Urban Design to ensure that any future development on the New Museums Site safeguards the architectural, historic, cultural and archaeological importance of the site while providing an opportunity for positive, qualitative change.

The draft SPD was approved for consultation at [Development Plan Scrutiny Sub-Committee on 16 June 2015](#)

A Sustainability Appraisal (which included the New Museums Site allocation<sup>2</sup>) and Habitats

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<sup>1</sup> Through the *Addendum to the Cambridge Local Plan 2014 Proposed Submission Document (July 2013): Schedule of Proposed Changes Following Proposed Submission Consultation (February 2014)*

<sup>2</sup> Cambridge City Council Part 2, 'Site Options within Cambridge' – Interim Sustainability Appraisal (2013)

Regulations Assessment Screening Report was carried out and consulted upon for the [Cambridge Local Plan 2014: Proposed Submission](#). This consultation took place between 19 July and 30 September 2013. These documents, along with other supporting documents were made available to view during the consultation.

A Habitats Regulations Assessment is an assessment to ensure that a plan will not have an adverse effect on the integrity of either Special Areas of Conservation (SAC) or Special Protection Areas (SPA).

As part of the Cambridge Local Plan 2014: Proposed Submission document preparation process, a Habitats Regulations Assessment was undertaken to assess the likely impact the Cambridge Local Plan may have. The assessment concluded that the Cambridge Local Plan is not likely to have any significant effects on the Natura 2000 or Ramsar sites identified.

For the New Museums Site SPD, a Sustainability Appraisal Screening Report was completed. This document confirmed that the allocation of land at the New Museums Site took place as part of the process of developing the Cambridge Local Plan 2014, and as such has been subject to a Sustainability Appraisal as part of the Local Plan process. The conclusion of this screening process is that as the New Museums Site SPD does not make any changes to this allocation, or the policy guidance contained within Policy 43 of the Plan, it will not give rise to significant environmental effects.

The New Museums Site SPD does not give rise to significant social and economic effects beyond those already identified as part of the appraisal of the parent policy and site allocation contained within the Cambridge Local Plan 2014. As such it is not considered necessary to undertake a separate Sustainability Appraisal for this SPD.

### **Consultees**

The following organisations (below) were directly notified of the draft New Museums Site Supplementary Planning Document (SPD) in accordance with the Town and Country Planning (Local Planning) (England) Regulations 2012 (as amended) via email or post where no email address was available (individuals are not listed).

## **SPECIFIC CONSULTATION BODIES<sup>3</sup>**

- Historic England
- Environment Agency
- Highways England
- Marine Management Organisation
- Natural England
- Network Rail
- Cambridgeshire Constabulary
- Cambridgeshire Police and Crime Commissioner
- Cambridge Fire and Rescue Service
- Cambridgeshire County Council (Highways)
- Cambridgeshire County Council (Strategic Planning)
- Comberton Parish Council
- Coton Parish Council
- Cottenham Parish Council
- Fen Ditton Parish Council
- Fulbourn Parish Council
- Girton Parish Council
- Grantchester Parish Council
- Great Shelford Parish Council
- Hauxton Parish Council
- Histon & Impington Parish Councils
- Horningsea Parish Council
- Madingley Parish

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<sup>3</sup> Specific consultation bodies and duty to cooperate bodies required under the Town and Country Planning (Local Planning) (England) Regulations 2012 as amended

- Council
- Milton Parish Council
- Orchard Park Community Council
- Teversham Parish Council
- National Grid
- UK Power networks
- Cambridgeshire Association to Commission Health
- Cambridgeshire and Peterborough Clinical Commissioning Group
- Cam Health
- Cambridge University Hospital NHS Foundations Trust (Addenbrooke's)
- NHS Property Services
- CATCH
- East of England Strategic Health Authority
- British Gas
- EDF Energy
- E.On Energy
- Npower Renewables
- Scottish Power
- Scottish & Southern Electric
- BT Openreach
- BT Group Plc
- Cambridge Water Company
- Anglian Water
- Homes and Communities Agency
- Office of Rail Regulation
- Civil Aviation Authority
- Greater Cambridge Greater Peterborough Enterprise Partnership
- Sport England

## **COUNCILLORS**

- 42 x City Councillors
- All County Councillors

(City Wards)

## **COMMUNITY ORGANISATIONS**

- Age Concern Cambridgeshire
- Cambridge Citizens Advice Bureau
- Cambridge Ethnic Community Forum
- Cambridge Federation of Residents' Associations
- Cambridge Interfaith Group
- Cambridgeshire Older Peoples Enterprise (COPE)
- Cambridgeshire Voluntary Sector Infrastructure Consortium (CVSIC)
- Camsight
- Disability Cambridgeshire
- East of England Faiths Council
- Encompass Network
- The Church of England Ely Diocese
- The GET Group

## **DEVELOPERS**

- Argyle Street Housing Cooperative
- Artek Design House
- Barratt Eastern Counties
- Barton Wilmore
- Beacon Planning Ltd
- Bellway Homes
- Berkeley Homes
- Bidwells
- Bovis Homes Ltd
- Brookgate
- Carter Jonas
- Chartered Institute of Architectural

- Technologists
- Cheffins
- Countryside Properties
- Estate Management and Building Service, University of Cambridge
- Grosvenor USS
- Home Builders Federation
- Januarys
- Liberty Property Trust
- RPS
- Savills
- Skanska UK Plc
- Unex

## **BUSINESSES**

- Marshalls Airport
- Cambridgeshire Chambers of Commerce
- Cambridge Network

## **EDUCATION**

- Anglia Ruskin University
- All Colleges of the University of Cambridge
- The Bursars' Committee

## **OTHERS**

- Cambridge Cycling Campaign
- Design Council/CABE
- Cambridgeshire Campaign for Better Transport



**Other methods of notification** included:

- A public notice in the Cambridge News;
- Through the council's webpages: <https://www.cambridge.gov.uk/new-museums-site>;
- Via facebook: <https://www.facebook.com/camcitco>;
- Twitter: <https://twitter.com/camcitco> and;
- The council's local plan blog: <http://cambridgelocalplan.wordpress.com/>.

**Consultation Methodology**

A six-week consultation period for the draft New Museums Site SPD took place from:

**9am on 13 July 2015 to 5pm on 7 September 2015**

The draft New Museums Site SPD was also made available for inspection along with the other relevant documents at the following locations:

- Online on the council's website <https://www.cambridge.gov.uk/new-museums-site-spd>;
- At the council's Customer Service Centre at Mandela House, 4 Regent Street, Cambridge, CB2 1BY from 8am-5.15 pm Monday and 9am-5.15pm Tuesday to Friday.

The draft New Museums Site SPD was also available for purchase from the Customer Service Centre (phone 01223 457200).

Comments could be made using:

- The online consultation system <http://cambridge.jdi-consult.net/localplan/> or;
- The printed response form which was available from Customer Service Centre (details above) or could be downloaded and completed electronically from the council's website <https://www.cambridge.gov.uk/new-museums-site-spd>

Completed forms could be returned to:

- Planning Policy, Cambridge City Council, PO Box 700, Cambridge, CB1 0JH
- Or emailed to [policysurveys@cambridge.gov.uk](mailto:policysurveys@cambridge.gov.uk)

Respondents could also request to be notified of the adoption of the document.

Contact details for further information were also made available as follows:

- Tel: 01223 457200
- Email: [policysurveys@cambridge.gov.uk](mailto:policysurveys@cambridge.gov.uk)

### **Key Issues Raised**

During the consultation, 63 representations were received. Of these, 4 (6%) were supportive, 23 (37%) were comments and the remainder, 36 (57%), were objections. The majority of responses came from the academic community however responses also came from a variety of other sources including local residents, businesses, planning consultants, the James Clerk Maxwell Foundation along with a number of other Clerk Maxwell enthusiasts, and Historic England.

In summary, the key issues raised included:

- The treatment of the site's permeability (approximately 30% of all the representations received);
- Concern over the treatment of, and alterations to, the Old Cavendish Laboratory and Mond buildings;
- Clarification and strengthening of particular paragraphs of the draft SPD;
- Historic England requested that minor changes to the text be made to clarify points of reference;
- Neighbouring organisations requested early consultation on proposals that may affect operational access and parking capacity/access.

The emerging Local Plan is still at the examination stage. If changes to the Local Plan 2014: Proposed Submission policies are made as part of the examination, the SPD will need to be updated to reflect these changes. For more information on the Local Plan examination, visit: <https://www.cambridge.gov.uk/local-plan-review-examination>.

The emerging Local Plan is still at the examination stage, which means that the council is unable to adopt the New Museums Site Development Framework as an SPD until the Local Plan has been found sound and adopted. In the interim period, prior to adoption of the SPD, the New Museums Site Development Framework provides context and guidance as material consideration in the planning process.